

Appendices

Survey Responses

National Register Listings

United States Department of the Interior
National Park Service



173

National Register of Historic Places Registration Form

This form is for use in nominating or requesting determinations for individual properties and districts. See instructions in National Register Bulletin, *How to Complete the National Register of Historic Places Registration Form*. If any item does not apply to the property being documented, enter "N/A" for "not applicable." For functions, architectural classification, materials, and areas of significance, enter only categories and subcategories from the instructions. Place additional certification comments, entries, and narrative items on continuation sheets (NPS Form 10-900a).

1. Name of Property

Historic name Marias River Bridge

Other names/site number 24TL401/MDT Identification No. L51306006+06001

2. Location

street & number Milepost 6 on Marias Valley Road (old US Highway 91) N/A not for publication

city of town Five miles south of Shelby vicinity

State Montana code MT county Toole code 101 zip code 59474

3. State/Federal Agency Certification

As the designated authority under the National Historic Preservation Act, as amended,

I hereby certify that this nomination request for determination of eligibility meets the documentation standards for registering properties in the National Register of Historic Places and meets the procedural and professional requirements set forth in 36 CFR Part 60.

In my opinion, the property meets does not meet the National Register Criteria. I recommend that this property be considered significant at the following level(s) of significance:

national statewide local

Signature of certifying official

Date

Title

State or Federal agency and bureau

In my opinion, the property meets does not meet the National Register criteria.

Signature of commenting official

Date

Title

State or Federal agency and bureau

4. National Park Service Certification

I, hereby, certify that this property is:

Signature of the Keeper

Date of Action

entered in the National Register

determined eligible for the National Register

determined not eligible for the National Register

removed from the National Register

other (explain:)

[Signature] 3/26/2012

Marias River Bridge
 Name of Property

Toole County, Montana
 County and State

5. Classification

Ownership of Property
 (Check as many boxes as apply)

Category of Property
 (Check only one box)

Number of Resources within Property
 (Do not include previously listed resources in the count.)

- Private
- public – Local
- public – State
- public - Federal
- Private

- building(s)
- district
- site
- structure
- building(s)
- object

Contributing	Noncontributing	
		buildings
		sites
1		structures
		Objects
		buildings
1	0	Total

Name of related multiple property listing
 (Enter "N/A" if property is not part of a multiple property listing)

Number of contributing resources previously listed in the National Register

Montana's Historic Steel Stringer and Steel Girder Bridges, 1901-1961

N/A

6. Function or Use

Historic Functions
 (Enter categories from instructions)

TRANSPORTATION/Road-related (vehicular) =
Bridge

Current Functions
 (Enter categories from instructions)

TRANSPORTATION/Road-related (vehicular) =
Bridge

7. Description

Architectural Classification
 (Enter categories from instructions)

OTHER: Steel Girder Bridge

Materials
 (Enter categories from instructions)

foundation: Concrete

walls: _____

roof: _____

other: Concrete, Metal: steel

Marias River Bridge

Name of Property

Toole County, Montana

County and State

Narrative Description

(Describe the historic and current physical appearance of the property. Explain contributing and noncontributing resources if necessary. Begin with a **summary paragraph** that briefly describes the general characteristics of the property, such as its location, setting, size, and significant features.)

Summary Paragraph

The Marias River Bridge consists of one contributing structure, a steel girder bridge built in 1936. The bridge is located on a segment of US Highway 91 now known as Marias Valley Road. The two-span structure is 545 feet long and 27 feet wide. It is representative of the type of steel girder bridges designed and built by the Montana Highway Department during the 1930s. There have been no significant changes to the structure since its construction and, with the exception of Interstate 15, there are no major changes in the setting of the property.

Narrative Description

The Marias River Bridge is located in north central Montana about five miles south of the City of Shelby on a bypassed segment of US Highway 91 that now serves as a frontage road. It is located on rolling grass plains comprised of the sedimentary sandstones and shales of the Fort Union Formation. The land was sculpted by ice sheets during the Bull Lake Ice Age about 70,000 years ago. The front range of the Rocky Mountains dominate the landscape to the west. The area surrounding the bridge is utilized for agricultural purposes.¹

The Marias River Bridge is a two-span steel girder structure with four steel stringer approach spans. The bridge has an overall length of 544 feet long and is 27 feet wide with a roadway width of 24 feet. There are two 120-foot girder spans. Each steel stringer span is 76 feet in length. The bridge ends rest on reinforced concrete abutments and the bridge spans rest on five solid reinforced concrete piers.

The steel superstructure of the bridge consists of two steel girder spans. Each span consists of four steel girders with two riveted girder spans reinforced with steel angle section stiffeners on the exterior sides of the bridge. The girders are connected by steel I-beam floor beams on each span. Angle section bottom lateral braces provide additional support for the spans. The concrete slab deck rests directly on the girders. The curbs and guardrails overhang the deck and are supported by steel I-beam braces. The concrete guardrails are double-coursed Art Deco style structures typical to Montana Highway Department-designed bridges between 1929 and 1941. They consist of double coursed concrete rails tilted at a 45° angle and supported by tapered concrete posts with beveled caps. The guardrails are anchored at the ends by concrete endposts with decorative recessed bush-hammered panels on the interior sides and three vertical grooves on the exterior sides. Steel I-beam guardrails have been bolted to the posts adjacent to the roadway.

The main spans are reached by four 76-foot steel I-beam stringer approach spans. The concrete decks are supported by eight lines of steel I-beam stringers. The approach spans display the same guardrails as the main spans.

Integrity

The Marias River Bridge retains excellent integrity. All of its original steel components are intact and unchanged. The bridge does suffer from some deterioration of the steel components (de-lamination and corrosion) and the concrete substructure. These problems, however, are common to bridges of this age. Sometime within the past 35 years, steel beam guardrails were bolted to the concrete guardrails adjacent to the roadway. The setting of the site is compromised somewhat by the close proximity of the Interstate 15 bridges over the Marias River.

¹ David Alt and Donald W. Hyndman, *Roadside Geology of Montana*, (Missoula: Mountain Press Publishing, 1991), 278.

Marias River Bridge
Name of Property

Toole County, Montana
County and State

8. Statement of Significance

Applicable National Register Criteria

(Mark "x" in one or more boxes for the criteria qualifying the property for National Register listing)

- A Property is associated with events that have made a significant contribution to the broad patterns of our history.
- B Property is associated with the lives of persons significant in our past.
- C Property embodies the distinctive characteristics of a type, period, or method of construction or represents the work of a master, or possesses high artistic values, or represents a significant and distinguishable entity whose components lack individual distinction.
- D Property has yielded, or is likely to yield, information important in prehistory or history.

Criteria Considerations

(Mark "x" in all the boxes that apply)

Property is:

- A owned by a religious institution or used for religious purposes.
- B removed from its original location.
- C a birthplace or grave.
- D a cemetery.
- E a reconstructed building, object, or structure.
- F a commemorative property.
- G less than 50 years old or achieving significance within the past 50 years.

Areas of Significance

(Enter categories from instructions)

Engineering

Transportation

Period of Significance

1936-1961

Significant Dates

1936

Significant Person

(Complete only if Criterion B is marked above)

Cultural Affiliation

Architect/Builder

Montana Highway Department

Thomas Staunton

Period of Significance (justification)

The Period of Significance for this historic property encompasses the year it was constructed through the historic period when it served as a component of US Highway 91 in north central Montana. The bridge is still in use.

Criteria Considerations (explanation, if necessary)

Marias River Bridge

Name of Property

Toole County, Montana

County and State

Statement of Significance Summary Paragraph (provide a summary paragraph that includes level of significance and applicable criteria)

The Marias River Bridge is eligible for listing on the National Register of Historic Places under criteria A and C. The bridge is eligible under Criterion A because of its association with the make-work New Deal programs of the Great Depression and the Montana Highway Department's attempt to modernize the state's roads utilizing federal funds available from that program. The bridge is also significant as an intact example of the highway department's standard steel girder bridge built from circa 1930 to 1941. The bridge retains all of its original components and features and is an excellent representative of the type.

Narrative Statement of Significance (provide at least one paragraph for each area of significance)

The Marias River Bridge is eligible for listing in the National Register of Historic Places under Criterion A for its association with the Montana Highway Department's road and bridge programs during the New Deal of the 1930s. The bridge was part of an extensive program by the highway department to improve and modernize Montana's transportation system utilizing federal funds during the Great Depression. The bridge was constructed under New Deal regulations and represents how the program was designed to maximize labor while minimizing the use of heavy machinery. The bridge has statewide significance because it represents the transition from the construction of steel truss structures to girder bridges that began during the Great Depression.

The bridge is also an excellent and intact example of the type of steel girder bridge designed and built by the Montana Highway Department in the 1930s. The bridge retains all of the structural components standard to steel girder bridges built during that period. The design reflects the pervasive Art Deco influence of the 1930s, specifically the concrete guardrails and overhanging deck. The bridge is an excellent example of the type and is eligible for the National Register under Criterion C.

Engineering Significance

The Marias River Bridge is an excellent example of the standard steel girder bridge designed and built by the Montana Highway Department during the 1930s. During the Great Depression, steel girder bridges began to replace steel truss structures as the highway department's preferred river crossing. The bridges were efficient and didn't have the height and width restrictions of the steel trusses. They were economical to construct and required less steel than the truss structures, which made them more attractive to the department's engineers. This structure retains all of the elements standard to the design including the deep, angle-section stiffened riveted exterior girders, the steel I-beam interior girders, floor beams, and concrete slab deck. The Art Deco-style concrete guardrails were standard to all steel girder, stringer, and concrete bridges from 1929 to 1941. The Marias River Bridge is representative of state-designed reinforced concrete T-beam bridges built between about 1929 and 1941.

Developmental history/additional historic context information (if appropriate)

On 30 August 1935, the Montana State Highway Commission awarded a contract to Great Falls contractor Thomas Staunton to build a "reinforced concrete and steel bridge over the Marias River on the Shelby – Conrad" section of US Highway 91 in Toole County. Staunton was one of six companies that bid on the project; he won it with a low bid of \$74,976. The Minneapolis Steel & Machinery Company supplied the structural steel for the bridge, while the Seattle-based Northwest Steel Rolling Mills provided the reinforcing steel for the piers and the concrete deck. Staunton completed the project on time and without incident sometime in 1936.²

Thomas Staunton

Born in England in 1878, Thomas Staunton was one of Montana's most active road and bridge builders in the 1930s. His family emigrated to the United States and settled in Minnesota in 1880. Staunton moved to Great Falls, Montana in 1906 and worked as an agent for the George L. Tracy Company, a wholesale grocer. His career as a contractor began about

² The construction file for this bridge has not survived. Montana State Highway Commission Meeting Minutes, Book 6, 334, 335 (30 August 1935); Bridge Condition Survey No. 151-0910-0029, Montana Department of Transportation, Helena, Montana.

Marias River Bridge

Toole County, Montana

Name of Property

County and State

1916 when he formed a partnership with James Fitzgerald. Fitzgerald & Staunton operated as general coal and railroad contractors until 1926, when they dissolved their association. For several years, Staunton was the vice president of F. J. Gies company, wholesale grocers and also had interests in the Kincaid Motor Company and two breweries in Spokane, Washington.³

Staunton began a six-year professional relationship with Evarts Blakeslee, a former SHC Resident Engineer and independent contractor, in 1936. The Staunton & Blakeslee company built several reinforced concrete bridges in Montana until 1942 when the partnership was dissolved because of a lack of local work caused by the demand of the Second World War. Possibly because of his bad experience with the Montana Highway Department's Milwaukee Road Railroad Overpass project in Missoula, Staunton purchased a cattle ranch south of Great Falls near Cascade, Montana in 1937. In 1943, he retired from the general contracting business to devote his full attention to his ranch. Thomas Staunton died in Great Falls after a lengthy illness in April 1956.⁴

9. Major Bibliographical References

Bibliography (Cite the books, articles, and other sources used in preparing this form on one or more continuation sheets)

Alt, David and Donald W. Hyndman. *Roadside Geology of Montana*. (Missoula: Mountain Press Publishing, 1991).

Axline, Jon. *Conveniences Sorely Needed: Montana's Historic Highway Bridges, 1860-1956*. (Helena: Montana Historical Society, 2005).

Bridge Condition Survey No. 151-0910-0029. Montana Department of Transportation. Helena, Montana.

Bridge Inspection Record No. L51306006+06001. Montana Department of Transportation. Helena, Montana.

Montana State Highway Commission Meeting Minutes. Montana Department of Transportation. Helena, Montana.

Previous documentation on file (NPS):

preliminary determination of individual listing (36 CFR 67 has been requested)
 previously listed in the National Register
 previously determined eligible by the National Register
 designated a National Historic Landmark
 recorded by Historic American Buildings Survey # _____
 recorded by Historic American Engineering Record # _____

Primary location of additional data:

State Historic Preservation Office
 Other State agency
 Federal agency
 Local government
 University
 Other

Name of repository: **Montana Department of Transportation**

Historic Resources Survey Number (if assigned): _____

³ "T. Staunton, Rancher and Businessman, Dies," *Great Falls Tribune*, April 9, 1956; Great Falls City Directory, 1906 - 1934.

⁴ During the construction of the Milwaukee Road Overpass in Missoula in 1936, Staunton refused to accede to the demands of the Missoula County Trade and Labor Council in regards to the wages paid his employees. In this case, the Labor Council had negotiated for a higher wage rate in Missoula County than what federal New Deal wage rates paid. After leaving a Missoula billiard parlor and café one evening in September 1936, Staunton and his time keeper were badly beaten by members of the local Teamsters Union. The injuries weren't life threatening, but did entail a hospital stay for the contractor and his colleague. See Milwaukee Road Railroad Overpass, Historic American Engineering Record No. MT-100, pages 10-11 (February 1999). Viewed at www.NPS.gov/habs_haer; "T. Staunton . . . Dies," *Great Falls Tribune*, April 9, 1956; Great Falls City Directory, 1935 - 1956.

Marias River Bridge
Name of Property

Toole County, Montana
County and State

10. Geographical Data

Acreage of Property 2.0
(do not include previously listed resource acreage)

UTM References

(Place additional UTM references on a continuation sheet)

1	<u>12</u>	<u>434122</u>	<u>5364180</u>	3	<u> </u>	<u> </u>	<u> </u>
	Zone	Easting	Northing		Zone	Easting	Northing
2	<u> </u>	<u> </u>	<u> </u>	4	<u> </u>	<u> </u>	<u> </u>
	Zone	Easting	Northing		Zone	Easting	Northing

Verbal Boundary Description (describe the boundaries of the property)

The boundary for the Marias River Bridge measures 545 x 35 feet and encompasses the bridge and its approaches on both sides of the river. The boundary is centered on the bridge.

Boundary Justification (explain why the boundaries were selected)

Boundaries for the Marias River Bridge are drawn to encompass the six spans of the bridge, its immediate approaches and that portion of the river spanned by the bridge. The width is increased beyond the measurements of the structure to include the piers and abutments.

11. Form Prepared By

name/title Jon Axline/Historian
organization Montana Department of Transportation date November 18, 2010
street & number 2701 Prospect Avenue telephone (406) 444-6258
city or town Helena state MT zip code 59620-1001
e-mail jaxline@mt.gov

Additional Documentation

Submit the following items with the completed form:

- **Maps:** A **USGS map** (7.5 or 15 minute series) indicating the property's location.
A **Sketch map** for historic districts and properties having large acreage or numerous resources. Key all photographs to this map.
- **Continuation Sheets**
- **Additional items:** (Check with the SHPO or FPO for any additional items)

Marias River Bridge
Name of Property

Toole County, Montana
County and State

Photographs:

Submit clear and descriptive black and white photographs. The size of each image must be 1600x1200 pixels at 300 ppi (pixels per inch) or larger. Key all photographs to the sketch map.

(See Continuation Sheets)

Property Owner:

(Complete this item at the request of the SHPO or FPO.)

name Montana Department of Transportation
street & number 2701 Prospect Avenue telephone 406-444-6200
city or town Helena state MT zip code 59602-1001

Paperwork Reduction Act Statement: This information is being collected for applications to the National Register of Historic Places to nominate properties for listing or determine eligibility for listing, to list properties, and to amend existing listings. Response to this request is required to obtain a benefit in accordance with the National Historic Preservation Act, as amended (16 U.S.C.460 et seq.).

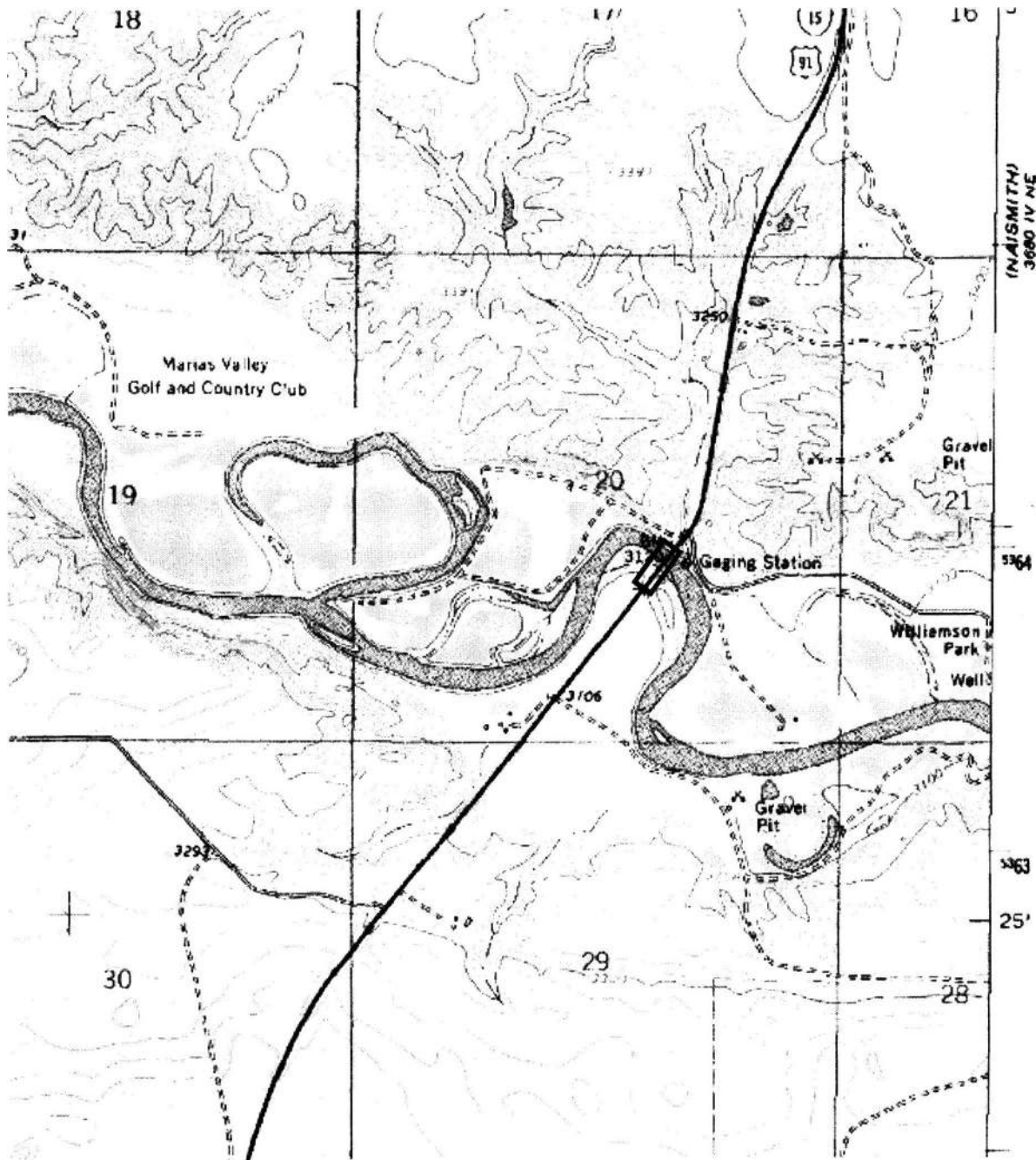
Estimated Burden Statement: Public reporting burden for this form is estimated to average 18 hours per response including time for reviewing instructions, gathering and maintaining data, and completing and reviewing the form. Direct comments regarding this burden estimate or any aspect of this form to the Chief, Administrative Services Division, National Park Service, PO Box 37127, Washington, DC 20013-7127; and the Office of Management and Budget, Paperwork Reductions Project (1024-0018), Washington, DC 20503.

United States Department of the Interior
National Park Service

National Register of Historic Places
Continuation Sheet

Marias River Bridge
Name of Property
Toole, MT
County and State
Montana's Historic Steel Stringer and Steel Girder Bridges, 1901-1961
Name of multiple listing (if applicable)

Section number Maps Page 9



Ledger NW, Montana USGS Quadrangle Map, 1971

**United States Department of the Interior
National Park Service**

**National Register of Historic Places
Continuation Sheet**

Marias River Bridge
Name of Property
Toole, MT
County and State
Montana's Historic Steel Stringer and Steel Girder Bridges, 1901-1961
Name of multiple listing (if applicable)

Section number Photographs Page 10

National Register Photographs

Name: Marias River Bridge
 County and State: Toole County, Montana
 Photographer: Kristi Hager
 Date of Photograph: 2005
 Location of original negative: Montana Department of Transportation. Helena, Montana.
 Description and view of camera: East elevation. View to the west
 Photograph: 0001

Name: Marias River Bridge
 County and State: Yellowstone County, Montana
 Photographer: Kristi Hager
 Date of Photograph: 2005
 Location of original negative: Montana Department of Transportation. Helena, Montana.
 Description and view of camera: West elevation. View to the northeast
 Photograph: 0002

Name: Marias River Bridge
 County and State: Yellowstone County, Montana
 Photographer: Kristi Hager
 Date of Photograph: 2005
 Location of original negative: Montana Department of Transportation. Helena, Montana.
 Description and view of camera: Detail. View to the east
 Photograph: 0003

United States Department of the Interior
National Park Service

National Register of Historic Places
Continuation Sheet

Marias River Bridge
Name of Property
Toole, MT
County and State
Montana's Historic Steel Stringer and Steel Girder Bridges, 1901-1961
Name of multiple listing (if applicable)

Section number _____ Photographs _____

Page 11

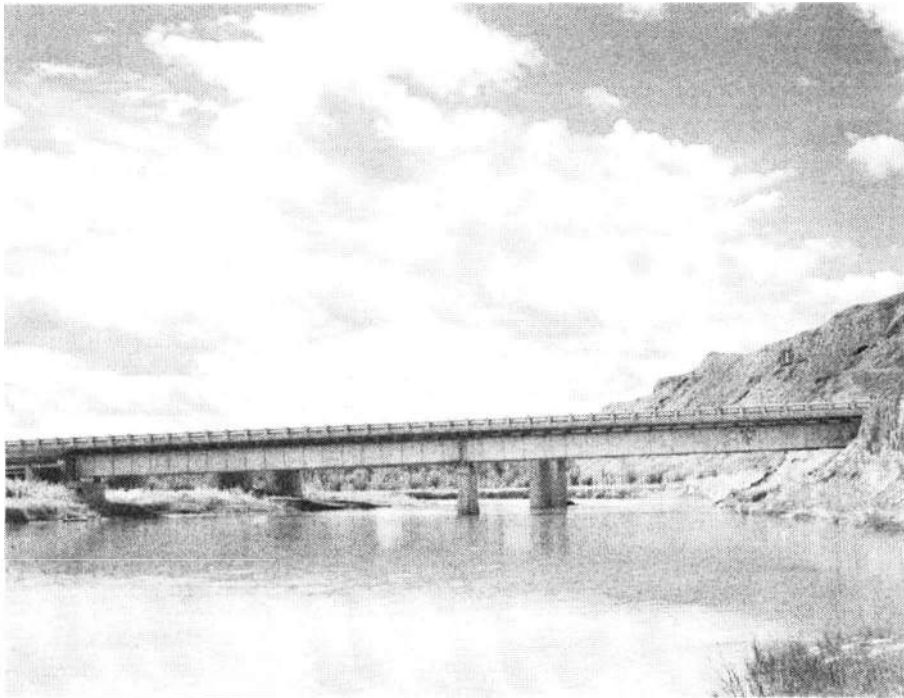


Photo 0001. Marias River Bridge. East elevation. View to the west



Photo 0002. Marias River Bridge. west elevation. View to the northeast

United States Department of the Interior
National Park Service

National Register of Historic Places
Continuation Sheet

Marias River Bridge

Name of Property

Toole, MT

County and State

Montana's Historic Steel Stringer and Steel
Girder Bridges, 1901-1961

Name of multiple listing (if applicable)

Section number _____ Photographs _____

Page 12



Photo 0003. Marias River Bridge. Detail. View to the east

UNITED STATES DEPARTMENT OF THE INTERIOR
NATIONAL PARK SERVICE

NATIONAL REGISTER OF HISTORIC PLACES
EVALUATION/RETURN SHEET

REQUESTED ACTION: NOMINATION

PROPERTY NAME: Marias River Bridge

MULTIPLE NAME: Montana's Steel Stringer and Steel Girder Bridges MPS

STATE & COUNTY: MONTANA, Toole

DATE RECEIVED: 2/17/12 DATE OF PENDING LIST: 3/09/12
DATE OF 16TH DAY: 3/26/12 DATE OF 45TH DAY: 4/04/12
DATE OF WEEKLY LIST:

REFERENCE NUMBER: 12000173

REASONS FOR REVIEW:

APPEAL: N DATA PROBLEM: N LANDSCAPE: N LESS THAN 50 YEARS: N
OTHER: N PDIL: N PERIOD: N PROGRAM UNAPPROVED: N
REQUEST: Y SAMPLE: N SLR DRAFT: N NATIONAL: N

COMMENT WAIVER: N

___ACCEPT ___RETURN ___REJECT _____DATE

ABSTRACT/SUMMARY COMMENTS:

The Marias River Bridge is significant under National Register Criteria A and C at the state and local levels in the areas of Transportation and Engineering. The bridge is an excellent and intact example of the type of steel girder bridge structures designed and built by the state highway department in Montana during the 1930s. Retaining its unique original features, the bridge served an important role in local transportation development. The resource meets the Registration Requirements set forth in the MPS cover.

RECOM./CRITERIA Accept Criteria A+C

REVIEWER Paul R. Lusignan DISCIPLINE HISTORIAN

TELEPHONE _____ DATE 3/26/2012

DOCUMENTATION see attached comments Y/N see attached SLR Y/N

If a nomination is returned to the nominating authority, the nomination is no longer under consideration by the NPS.



Marias River Bridge
Toole Co., MT
#0001



Marías River Bridge

Toole Co., MT

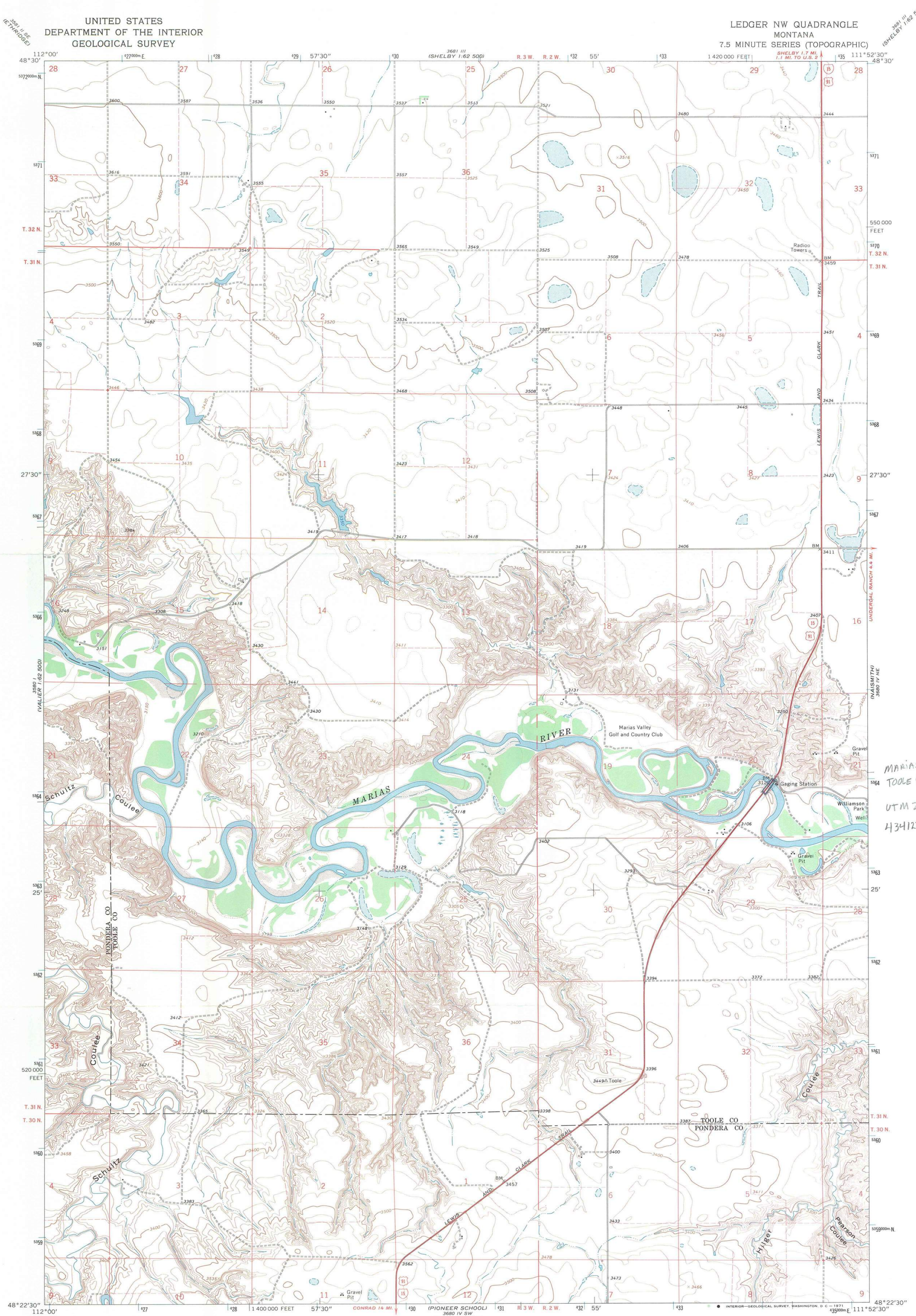
0002



Marias River Bridge

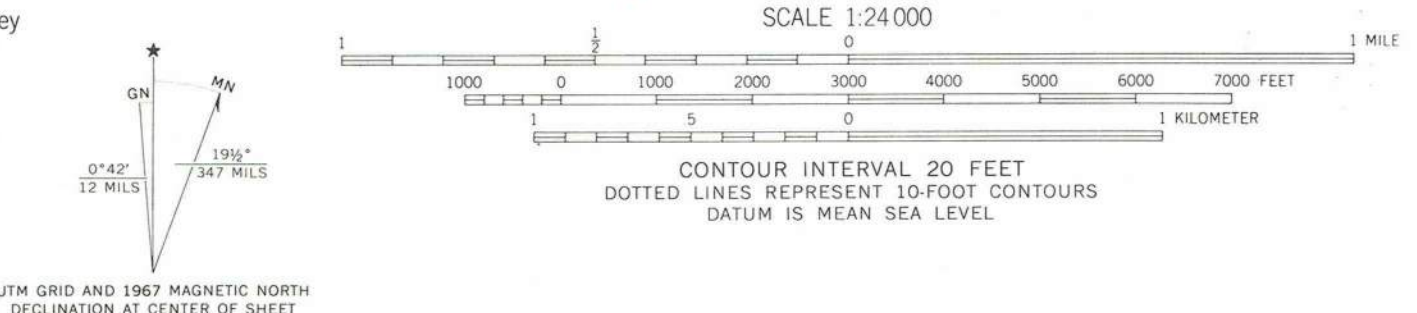
Toole Co., MT

0003



MARIAS RIVER BRIDGE
TOOLE COUNTY, MONTANA
UTM ZONE 12
434122E 5364180 N

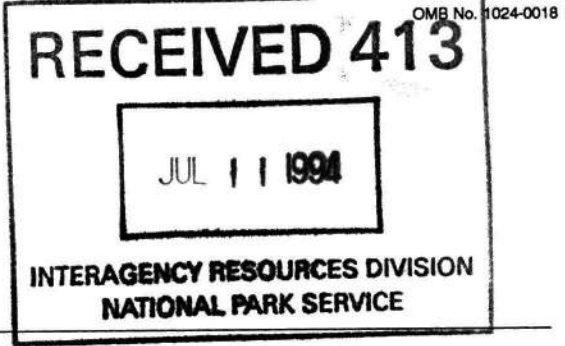
Mapped, edited, and published by the Geological Survey
Control by USGS and USC&GS
Topography by photogrammetric methods from aerial
photographs taken 1965. Field checked 1967
Polyconic projection. 1927 North American datum
10,000-foot grid based on Montana coordinate system,
north zone
1000-meter Universal Transverse Mercator grid ticks,
zone 12, shown in blue
Fine red dashed lines indicate selected fence lines



THIS MAP COMPLIES WITH NATIONAL MAP ACCURACY STANDARDS
FOR SALE BY U.S. GEOLOGICAL SURVEY, DENVER, COLORADO 80225, OR WASHINGTON, D. C. 20242
A FOLDER DESCRIBING TOPOGRAPHIC MAPS AND SYMBOLS IS AVAILABLE ON REQUEST

LEDGER NW, MONT.
N4822.5-W1152.5/7.5
1967
AMS 3680 IV NW-SERIES V894

8660



United States Department of the Interior
National Park Service

NATIONAL REGISTER OF HISTORIC PLACES
REGISTRATION FORM

1. Name of Property

historic name: Rainbow Conoco

other name/site number: Joe's

2. Location

street & number: 400 Main Street

not for publication: n/a
vicinity: n/a

city/town: Shelby

state: Montana

code: MT

county: Toole

code: 101

zip code: 59474

3. State/Federal Agency Certification

As the designated authority under the National Historic Preservation Act of 1986, as amended, I hereby certify that this X nomination request for determination of eligibility meets the documentation standards for registering properties in the National Register of Historic Places and meets the procedural and professional requirements set forth in 36 CFR Part 60. In my opinion, the property X meets does not meet the National Register Criteria. I recommend that this property be considered significant nationally statewide X locally. (See continuation sheet for additional comments.)

Maurell Shelby MT SHPO 6-24-94
Signature of certifying official/Title Date

Montana State Historic Preservation Office
State or Federal agency or bureau

In my opinion, the property meets does not meet the National Register criteria.

Signature of commenting or other official Date

State or Federal agency and bureau

4. National Park Service Certification

I, hereby certify that this property is:

 entered in the National Register

 see continuation sheet

 determined eligible for the
National Register

 see continuation sheet

 determined not eligible for the
National Register

 see continuation sheet

 removed from the National Register

 see continuation sheet

 other (explain): _____

for
Signature of the Keeper Date of Action
Edson H. Beal Entered in the
National Register 8.16.94

5. Classification

Ownership of Property: Private

Number of Resources within Property

Contributing Noncontributing

Category of Property: Building

 1 building(s)

Number of contributing resources previously
listed in the National Register: 0

 sites

 structures

 objects

Name of related multiple property listing:

Roadside Architecture Along US Highway 2 in Montana 1

 TOTAL

6. Function or Use

Historic Functions:

Current Functions:

Commerce/Trade: specialty store

Commerce/Trade: specialty store

7. Description

Architectural Classification:

Materials:

Other: Domestic style gas station

foundation: concrete

walls: brick

roof: asphalt

other: n/a

Narrative Description

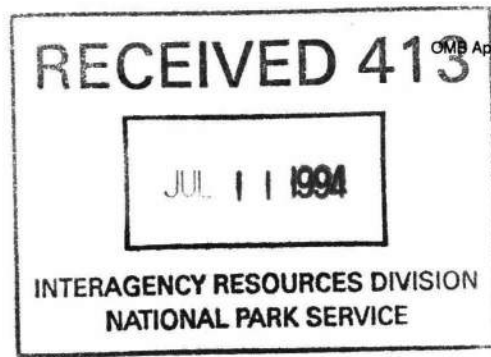
Rainbow Conoco is located at the southwest corner of the intersection of Main Street and Third Avenue in the central business district of Shelby, Montana. The business faces Main Street. The original building was constructed in 1936 and designed by Continental Oil Company architects in accordance with prevailing corporate architectural policy during the mid-1930s. In 1941 the company added a second service bay to the northwest end of the original structure in response to a request by Mr. Leroy Whitney, the local lessee. The service business had increased substantially in the late 1930s and required additional indoor service space to handle the winter work load. This early expansion was exceptionally well integrated to the 1936 building, to the point where one has to look very carefully to detect the addition. Conoco sold the building in 1965 to a local bulk oil distributor, Howard Mitchell, who in turn, sold the building to station operator Joe Kincaid in 1975. Mr. Kincaid immediately erected another shop addition, a twenty-nine by thirty-five foot, flat roof metal structure that adjoins the earlier 1941 service bay. There was no attempt to architecturally integrate the 1975 construction with the original design.

The original building (including the 1941 addition) is a Continental Oil Company adaption of Pure Oil's classic domestic form known as the English cottage-type station. Conoco's version retains the steeply pitched gable roof and perpendicular gabled service bays but eliminates the slate shingles and bay windows used by most competing corporations during the 1930s. The off-white glazed brick exterior contrasts nicely with the dark green asphalt shingle roof. Orange-red natural brick trim surrounds the centrally positioned metal plate glass office door. Two almost square, fixed single pane windows flank the front entry while another identical window is located on the northeast side of the office. There are also three smaller four pane windows and a wood door along the rear and southeast side of the office portion of the complex. All windows also are highlighted in red-orange brick above and below the metal casements. The men's room is accessed from

See continuation sheet

United States Department of the Interior
National Park Service

**National Register of Historic Places
Continuation Sheet**



Section number 7

Rainbow Conoco

Page 1

the office while the ladies' room is entered from the back of the building. Both rest rooms retain their original multi-colored tile trim.

Once inside the service area, it is easier to see that the far west bay was not an integral part of the original building. The bays are connected by a large central passageway that was created by removing the majority of the original west wall. Dark cream-colored tiles line the interior of both bays.

As was common to most pre-1940 service stations, office space is quite restricted. A couple of merchandising racks, soda and coffee machines, and a small desk and cash register area fill the room to capacity. Lubricants are stored in the adjacent service area.

The 1975 metal addition is an entirely different design and was never intended to be an integral part of the historic station. It was built to house an ancillary major repair business. This venture lasted only a couple of years and today the building is used mostly for storage. Built simply alongside the original station, the addition reads as almost a separate building, lessening the visual impact of its insensitive design. Fortunately, the historical portions of the Rainbow Conoco building is very well preserved, allowing this still functioning station to convey strongly its original design and historic associations.

8. Statement of Significance

Applicable National Register Criteria: A, C Areas of Significance: Architecture
Commerce

Criteria Considerations (Exceptions): n/a Period(s) of Significance: 1936-1944

Significant Person(s): n/a Significant Dates: 1936, 1941

Cultural Affiliation: n/a Architect/Builder: Continental Oil Company

Narrative Statement of Significance

Rainbow Conoco is one of finest examples of 1930s era gas station design in Montana. Equally impressive is the fact that the business has operated continuously for nearly sixty years, providing three generations of motorists with courteous, dependable service. Joe Kincaid, the present owner, has operated the business for twenty-eight years. Joe's, as the station is affectionately called by most local residents, is a Shelby commercial landmark. Few businesses in the area have been owned and operated by the same individual for as long as Mr. Kincaid has managed Rainbow Conoco.

The building is the best original domestic, English cottage design remaining in the State. The styling reflected the desire to avoid conflict with nearby residential environments and blend effectively with adjacent commercial structures. The beautifully maintained glazed brick exterior is accentuated with natural brick trim around windows and doors in the classic 1930s style. The high-pitched gable roof and distinctive white and green color scheme made the station a familiar and recognizable landmark to passing motorists since Continental Oil used a similar design for virtually all of its company outlets. The architectural integrity of this particular station is remarkable. With the exception of signage, computer gas pumps, and a newer style, metal office door, virtually the entire structure has remained untouched for over fifty years. According to the present owner, the building was designed and built by the Continental Oil Company in 1936.¹ The station opened for business in early April and was first operated by Obert Knudsen. Mr. Knudsen was replaced by a Mr. Neilsen about a year and a half later, who in turn, was succeeded by Leroy Whitney. From the early 1940s until about 1960, Leroy Gunlikson, and after the war, his son Jim, leased and operated the station. Art Rossman acquired the business after the Gunlikson's. In 1965 the present owner, Joe Kincaid began an almost thirty year career with Conoco.

Continental Oil (Conoco) is one of a very few pre-Second World War era petroleum corporations still conducting business in Montana under the same corporate logo.² Rainbow Conoco has been an exclusive outlet for Continental Oil Company products during its entire commercial life. The business certainly must be one of corporation's oldest continuously operated stations in the region.

¹The Continental Oil Company has been known as Conoco since at least the late 1920s.

²Conoco may be the only 1920s era company still doing in the State. Both Texaco and Standard Oil of Indiana left Montana in the early 1980s.

9. Major Bibliographic References

Bayer, Linda. "Roadside Architecture," Historic Huntsville Quarterly 9:1.2 (1983).
 Gebhard, David and Gerald Mansheim. Buildings of Iowa. Oxford University Press, New York (1993).
 Henderson, Wayne. Guide to Gasoline Logos. self-published, Marshall, NC (1993).
 Jakle, John. "The American Gas Station, 1920-1970," Journal of American Culture Spring (1976).
 Ketcham, Linda. Shelby resident for many years, personal communication, Bozeman, MT (1993).
 Kincaid, Joe. Manager and owner of Rainbow Conoco, personal communication, Shelby, MT (August, 1993).
 Liebs, Chester. Main Street to Miracle Mile. Little, Brown & Company, Boston (1985).
 Suter, Scott H. "Neither Weird nor Elegant: The Evolution of the Cottage-Style Gas Station," Society for Commercial Archeology News Journal 12:3 (1993).
 Vieyra, Daniel. Fill'er Up. Collier Books, New York (1978).

Previous documentation on file (NPS):

- preliminary determination of individual listing (36 CFR 67) has been requested.
- previously listed in the National Register
- previously determined eligible by the National Register
- designated a National Historic Landmark
- recorded by Historic American Buildings Survey # _____
- recorded by Historic American Engineering Record # _____

Primary Location of Additional Data:

- State Historic Preservation Office
- Other State agency
- Federal agency
- Local government
- University
- Other -- Specify Repository:

10. Geographical Data

Acreage of Property: less than one acre

UTM References:	Zone	Easting	Northing
	A 12	436575	5372720

Legal Location (Township, Range & Section(s)): SW ¼, NW ¼, NW ¼ of Section 27, T32N, R2W

Verbal Boundary Description

Shelby Townsite, Block 17, Lot 1.

Boundary Justification

The nominated property includes the lot upon which the nominated building is located.

11. Form Prepared By

name/title: Joseph M. Ashley, Associate Professor of Geography
 organization: Montana State University date: October 1993
 street & number: Dept of Earth Sciences telephone: 406/994-6904
 city or town: Bozeman state: MT zip code: 59717

Property Owner

name/title: Joe Kincaid
 street & number: Box 892
 city or town: Shelby state: MT zip code: 59474

UNITED STATES DEPARTMENT OF THE INTERIOR
NATIONAL PARK SERVICE

NATIONAL REGISTER OF HISTORIC PLACES
EVALUATION/RETURN SHEET

REQUESTED ACTION: NOMINATION

PROPERTY NAME: Rainbow Conoco

MULTIPLE NAME: Roadside Architecture Along US 2 in Montana MPS

STATE & COUNTY: MONTANA, Toole

DATE RECEIVED: 7/11/94 DATE OF PENDING LIST: 7/28/94
DATE OF 16TH DAY: 8/13/94 DATE OF 45TH DAY: 8/25/94
DATE OF WEEKLY LIST:

REFERENCE NUMBER: 94000866

NOMINATOR: STATE

REASONS FOR REVIEW:

APPEAL: N DATA PROBLEM: N LANDSCAPE: N LESS THAN 50 YEARS: N
OTHER: N PDIL: N PERIOD: N PROGRAM UNAPPROVED: N
REQUEST: N SAMPLE: N SLR DRAFT: N NATIONAL: N

COMMENT WAIVER: N

ACCEPT RETURN REJECT 8.16.94 DATE Entered in the
National Register

ABSTRACT/SUMMARY COMMENTS:

RECOM./CRITERIA _____
REVIEWER _____
DISCIPLINE _____
DATE _____

DOCUMENTATION see attached comments Y/N see attached SLR Y/N

CLASSIFICATION

count resource type

STATE/FEDERAL AGENCY CERTIFICATION

FUNCTION

historic current

DESCRIPTION

architectural classification
 materials
 descriptive text

SIGNIFICANCE

Period Areas of Significance--Check and justify below

Specific dates Builder/Architect
Statement of Significance (in one paragraph)

summary paragraph
 completeness
 clarity
 applicable criteria
 justification of areas checked
 relating significance to the resource
 context
 relationship of integrity to significance
 justification of exception
 other

BIBLIOGRAPHY

GEOGRAPHICAL DATA

acreage verbal boundary description
 UTM's boundary justification

ACCOMPANYING DOCUMENTATION/PRESENTATION

sketch maps USGS maps photographs presentation

OTHER COMMENTS

Questions concerning this nomination may be directed to

_____ Phone _____

Signed _____ Date _____



Rainbow Conoco
400 Main St
Toole County
Shelby, MT

photographer: Joseph M. Ashley

date: Sep 1993

neg: SHPO, Helena, MT

view SW

#1



UNLEADED

1.38⁹

LEADED

1.48⁹

Rainbow Cmoco
400 Main St
Toole County
Shelby, MT

photographer: Joseph M. Ashley

date: Sep 1993

neg: 34PO, Helena, MT

view: WSW

#2



Rainbow Conoco
400 Main St
Toole County
Shelby, MT

photographer: Joseph M. Ashley

date: Sep 1993

neg: SH PO, Helena, MT

view: NNW

#3



Rainbow Conoco
400 Main Street
Toole County
Shelby, MT

photographer: Joseph M. Ashley

date: Sep 1993

neg: SHPO, Helena, MT

view: SE

#4



Rainbow Conoco 1975 addition
400 Main St
Toole County
Shelby, MT

photographer: Joseph M. Ashley

date: Sep 1993

neg: 54 PD, Helena, MT

view SSE

#5



Rainbow Conoco owner Joe Kincaid
400 Main St
Toole County
Shelby, MT

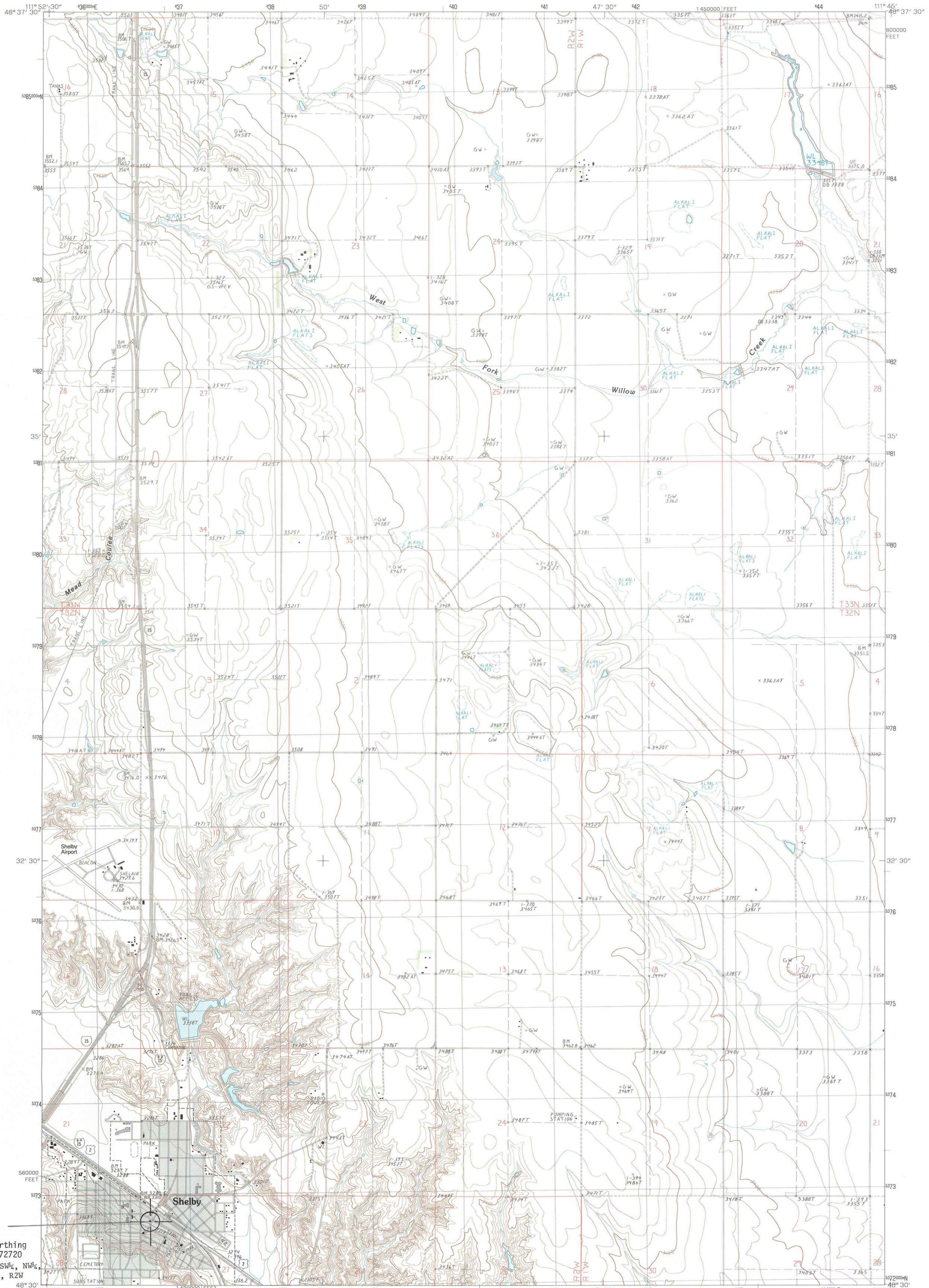
photographer: Joseph M. Ashley

date: Sep 1993

neg: 54PO, Helena, MT

view WSW

#6



Rainbow Conoco
UTM References
Zone Easting Northing
12 436575 5372720
Located in the SW¹/₄, NW¹/₄,
NW¹/₄ Sec 8, T32N, R2W
Shelby
Toole County, Montana

PRODUCED BY THE UNITED STATES GEOLOGICAL SURVEY
CONTROL BY USGS AND NOS/NOAA
COMPILED FROM AERIAL PHOTOGRAPHS TAKEN 1983
FIELD CHECKED 1984. MAP EDITED 1986
PROJECTION LAMBERT CONFORMAL CONIC
GRID: 1000-METER UNIVERSAL TRANSVERSE MERCATOR
ZONE 12
10,000-FOOT STATE GRID TICKS MONTANA, NORTH ZONE
UTM GRID DECLINATION 0°37' WEST
1986 MAGNETIC NORTH DECLINATION 18°00' EAST
VERTICAL DATUM NATIONAL GEODETIC VERTICAL DATUM OF 1929
HORIZONTAL DATUM 1927 NORTH AMERICAN DATUM
To place on the predicted North American Datum 1983,
move the projection lines as shown by dashed corner ticks
(8 meters north and 62 meters east)
There may be private inholdings within the boundaries of any
Federal and State reservations shown on this map
No distinction made between houses, barns, and other buildings
Gray tint indicates area in which selected buildings are shown

PROVISIONAL MAP
Produced from original
manuscript drawings. Informa-
tion shown as of date of
photography.



CONTOUR INTERVAL 10 FEET
CONTROL ELEVATIONS SHOWN TO THE NEAREST 0.1 FOOT
OTHER ELEVATIONS SHOWN TO THE NEAREST FOOT
To convert feet to meters multiply by .3048
To convert meters to feet multiply by 3.2808

THIS MAP COMPLIES WITH NATIONAL MAP ACCURACY STANDARDS
FOR SALE BY U.S. GEOLOGICAL SURVEY, DENVER, COLORADO 80225
OR RESTON, VIRGINIA 22092

QUADRANGLE LOCATION

1	2	3
4	5	6
7	8	

ADJOINING 7.5' QUADRANGLE NAMES

ROAD LEGEND

Improved Road
Unimproved Road
Trail

Interstate Route U.S. Route State Route

1 Keith South
2 Ollmont
3 Antelope Coulee NW
4 Sinden
5 Antelope Coulee SW
6 Ledger NW
7 Nalworth
8 Dunkirk

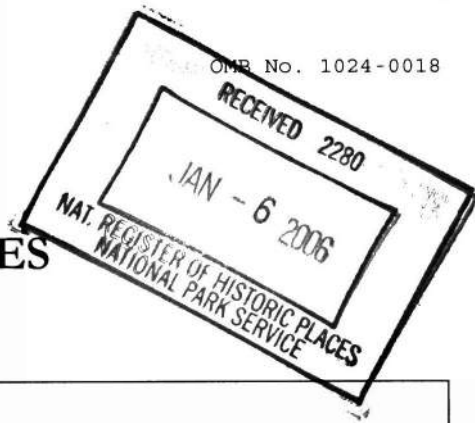
SHELBY, MONTANA
PROVISIONAL EDITION 1986

4811-E7-TF-024

From the Library of
DEPARTMENT OF EARTH SCIENCES
Montana State University

United States Department of the Interior
National Park Service

NATIONAL REGISTER OF HISTORIC PLACES REGISTRATION FORM



1. Name of Property

historic name: Shelby Town Hall
other name/site number: Shelby Chamber of Commerce/Visitor Information Center

2. Location

street & number: 100 Montana Ave. not for publication: n/a
city/town: Shelby vicinity: n/a
state: Montana code: MT county: Toole code: 101 zip code: 59474

3. State/Federal Agency Certification

As the designated authority under the National Historic Preservation Act of 1986, as amended, I hereby certify that this nomination request for determination of eligibility meets the documentation standards for registering properties in the National Register of Historic Places and meets the procedural and professional requirements set forth in 36 CFR Part 60. In my opinion, the property meets does not meet the National Register Criteria. I recommend that this property be considered significant nationally statewide locally.

John Wilmoth 1/2/06
Signature of certifying official/Title Date

Montana State Historic Preservation Office
State or Federal agency or bureau (See continuation sheet for additional comments.)

In my opinion, the property meets does not meet the National Register criteria.

Signature of commenting or other official Date
State or Federal agency and bureau

4. National Park Service Certification

I, hereby certify that this property is:
 entered in the National Register
 see continuation sheet
 determined eligible for the National Register
 see continuation sheet
 determined not eligible for the National Register
 see continuation sheet
 removed from the National Register
 see continuation sheet
 other (explain):

for
Signature of the Keeper
Edson H. Beall
Date of Action
2/14/06

5. Classification

Ownership of Property: Public-local	Number of Resources within Property	
	Contributing	Noncontributing
Category of Property: Building	<u>1</u>	<u>0</u> buildings
	<u>0</u>	<u>0</u> sites
Number of contributing resources previously listed in the National Register: n/a	<u>0</u>	<u>0</u> structures
	<u>0</u>	<u>0</u> objects
Name of related multiple property listing: n/a	<u>1</u>	<u>0</u> Total

6. Function or Use

Historic Functions:
GOVERNMENT – City Hall

Current Functions:
GOVERNMENT – Municipal Building

7. Description

Architectural Classification:
LATE 19TH AND EARLY 20TH CENTURY
AMERICAN MOVEMENTS -
Craftsman (commercial)

Materials:
foundation: CONCRETE
walls: STUCCO
roof: OTHER - tar
other: METAL

Narrative Description

Constructed in 1923, the one-story, rectangular Shelby Town Hall is situated on a grassy lot at the north-west corner of the original Shelby Town Square, on the east end of Main Street. Its wood framing rests on a concrete wall foundation, and is covered by stucco on each of its exterior walls. The slope of the property reveals a full daylight basement at the north (side) and east (rear) elevations. Painted white with Williamsburg blue highlights, this modern paint pattern gives the appearance of an Art Deco style, but the building in fact dates to the arts and crafts period, when historic multi-tonal painting emphasized the horizontality of the building, accented by raised banding.

The west elevation serves as the façade, and welcomes visitors from Main Street. A metal ramp to the south and concrete steps to the north both lead to a full-width concrete stoop. A semi-circular modern awning shelters the stoop and reads CHAMBER OF COMMERCE across the front. Four decorative pilasters are spaced evenly across the façade, one on either side of the centered entry, and one at each corner. These pilasters extend beyond the short parapet wall at the roofline and effectively divide the elevation into three bays. They are topped with decorative banding, chevrons, and rounded caps. The flanking bays each contain a centered, metal-framed, one-light fixed window. Within the parapet wall above the door is a modern sign that reads: VISITOR INFORMATION CENTER. The flanking bays of the parapet wall each contain a raised, rectangular panel.

The north (side) elevation features two levels: the main floor and the daylight basement. Pilasters define the space at the corners. The main story displays three original window openings, all containing modern, vertically divided, metal-framed windows. The central opening is narrower and contains two-lights, while the east and west openings each contain four ribboned lights. Within the parapet wall, a raised rectangular panel is located above each of the window openings. A modern, centered, one-light metal door, protected by a metal storm, provides entry below grade at the basement level. Centered on the wall on either side of the door is a large, modern, two-light, metal-framed sliding window. A concrete ramp leads down to the door from the northeast corner of the building.

Fenestration on the south (side) elevation is limited to a single, centered modern window. The parapet wall steps down twice from east to west. In 2005, the historic small entry extension on the west (rear) elevation was expanded to accommodate handicapped accessible bathrooms. The two-story, stuccoed, wood-frame addition features entry doors at the basement level, and a very shallow gabled roof.

(see continuation sheet)

**United States Department of the Interior
National Park Service**

**National Register of Historic Places
Continuation Sheet**

Section number 7

Shelby Town Hall
Toole County, MT

Page 1

Interior:

Not much is known of the original interior of the Town Hall as council minutes only touch on the bids for upkeep and maintenance. Conversations with several long-time Shelby residents indicate the main level of the building had several smaller office spaces and a restroom. The October 17, 1938 council minutes record a WPA Project to remodel the building. Plans were made to tear out partitions to make one large room in the front of the building; build a small storm vestibule around the outside of the front door; repair the roof and install small floor furnaces.

The basement had several rooms designed for meeting rooms. Several groups included a woman's club and the fire department requested space in the basement as clubrooms and a library in the winter months. Two years after the original construction, bids were advertised for two cells to hold four prisoners creating a jail in the basement of the Shelby Town Hall.

Today the Shelby Town Hall serves as a Visitor Information Center. The main level now is a large open space with a counter on the north side of the room separating the Chamber Office from the visitors' section. A partial wall divides the room running east-west. The south side of the room is office space for the Visitor Information director and restrooms. The front receiving area is furnished in a western flair for visiting tourists. On display is a mountain lion captured and killed in the Shelby area in 1996, display cases with Montana products, brochures and magazines for travelers. The basement of the building is now used as additional office space for city personnel.

The building is unique in its architectural design. The foundation is of concrete construction. Exterior walls; standard wood framed, were finished with a rough stucco/plaster, with prominent vertical pilasters accenting the corners and doorway of the building. Historic photos indicate a dark multi-colored exterior with light-colored trim and accents. Its original four-over-one double-hung windows and horizontal banding of color is indicative of Craftsman style as well, more in line with the type of designs Frank Bossuot was known for creating. The roof of the building is flat, constructed with timber beams and a tar and paper covering.

Integrity:

The building has been altered through its history, including a 1938 enclosure surrounding the original entrance and interior reconfiguration sponsored by the Works Progress Administration. Non-historic changes include the loss of original windows and doors, resulting in diminished integrity of design, workmanship and materials. However, the property retains sufficient integrity to convey its associations with community development and government in Shelby. It retains excellent integrity of feeling, setting, location, and association.

8. Statement of Significance

Applicable National Register Criteria: A
Criteria Considerations (Exceptions): n/a
Significant Person(s): n/a
Cultural Affiliation: n/a

Areas of Significance: POLITICS/GOVERNMENT
Period(s) of Significance: 1923-1955
Significant Dates: 1923
Architect/Builder: Bossuot, Frank - Architect
Humphrey, J.P. - Builder

Narrative Statement of Significance

Constructed in early spring of 1923, the Shelby Town Hall served as city offices, a local citizens meeting hall and a few years later, a jail for the bustling oilfield community. The Town Hall was quite modern considering the streets were dirt and sewer and electrical lines were just being laid in the community. The building was constructed in only two months anticipating the “droves” of fight fans and media who would be flocking to Shelby for the infamous “World Heavyweight Championship Fight of 1923.” The building was to be the headquarters for the media men who would be covering the big event and a symbol of the progress of Shelby. More importantly, it was built to serve the growing needs of a small, bustling community just beginning to lay its roots. Through the twentieth century, the building served the local government needs of the town, and served as the center of political activity. The Shelby Town Hall is historically significant under National Register Criterion A for its association with the development of Shelby and local government.

Long the territory of the Blackfeet, the front range of the Rocky Mountains boasts a rich history. The Gros Ventre, Blackfeet, Salish, Kootenai, Assiniboine and the Plains Cree crossed the plains to their sacred grounds in the Sweetgrass Hills, approximately 50 miles northeast of Shelby. The Hills have a significant traditional, historical and ceremonial value to the Native Americans of this area. The native Blackfeet and Blood names for the Sweetgrass Hills translates to “Place of the Sweet Pine”, “Sweet Pine”, “Sweet Pine Hills” or “Pine Needles Buttes”, but the name was mistranslated into “Sweetgrass Hills”. During the 1880s, gold was discovered there, enticing hundreds of miners to the area in search of the precious metal. By 1886, 400-500 miners had come into the area.

The city of Shelby itself had quite meager beginnings. Shelby was born with the coming of the Great Northern Railway in 1891. The railway builders threw off a boxcar at the site and named it Shelby Junction for Peter P. Shelby, then general manager in Montana for Jim Hill’s railroad. Having been so honored, the general manager is said to have remarked: “That mudhole, God-forsaken place will never amount to a damn!” Despite Mr. Shelby’s prediction, Shelby became the distribution center for a sizeable trade area with the coming of the Great Northern. It was a typical cowtown in which cowboys and sheepmen gathered for supplies and revelry. Within a short time, wheat fields began to spring up across the prairie as homesteaders followed the cowboys into the area.

Following the boom of World War I, there came the dry year of 1919, and the vast area around Shelby suffered a time of drastic drought and financial turmoil. Real estate and livestock valuations sank, and the many small banks which had freely granted loans were being pressed for payment by eastern lenders. So desperate was the situation that a national magazine published an article about it entitled, “The Pain in the Northwest”.

Almost overnight, the discovery of oil in the Kevin-Sunburst fields by Gordon Campbell in March of 1922 sparked an incredible economic boom for Toole County and the county seat, Shelby. Shelby was being hailed as the “Tulsa of the West”. Headlines cited Shelby as the fastest growing city in the U.S. With this tremendous influx of oilfield workers and their families, Shelby businessman, James A. Johnson and his son, James W. (Body) Johnson set to work selling lots and buildings, small office spaces, houses and apartments to meet the new demand. The real estate market boomed that summer and into late fall of 1922.

As 1922 drew to a close and a long winter ensued in northern Montana, the real estate market slowed down. Ever the shrewd businessman, James A. Johnson, feeling the economic turndown, creatively hatched the “big fight” idea as a publicity stunt to bring renewed attention to Shelby and hopefully spur business. Not surprising, businessman James A. Johnson was also mayor of the City of Shelby. Likely in anticipation of the event, he convinced the city council to take the necessary steps to build the Shelby Town Hall, establishing Shelby as a city of growth and perpetuity. Plans were drawn up for a one-story building with a full basement. On April 4, 1923, Mr. Frank Bossuot, an architect from Havre, Montana was hired to design the government building.¹ “Time being of the essence,” in less than two weeks, at the April 16th council meeting, builder J.P. Humphrey was awarded the bid.

(see continuation sheet)

¹ Boussuot designed the Phillips County Courthouse (1917), Hill County Courthouse (1915), and Judith Basin County Courthouse (1925).

United States Department of the Interior
National Park Service

National Register of Historic Places
Continuation Sheet

Section number 8

Shelby Town Hall
Toole County, MT

Page 2

Mr. Humphrey's bid of \$4920.00 was accepted over the McLean Brothers bid of \$4,700.00, as the McLean Brothers would not accept the city's general fund warrants in payment. Work began immediately on the Shelby Town Hall.

Jack Dempsey v. Tommy Gibbons

Information regarding the fight was taken from "History of Shelby, Montana," online at <http://www.bigskyfishing.com/Montana-Info/Hi-Line/history-hi-line.shtm>.

After seeing a headline in a Great Falls paper of how Montreal was offering a \$100,000 purse for a boxing match between Heavyweight Champion Jack Dempsey and a yet unnamed challenger, an enterprising individual named James W. "Body" Johnson, who wanted to call attention to his own land holdings, came up with the unique idea to up the offer a bit. The hopes were that by offering a sky high proposal to bring a professional boxing match featuring Dempsey to Shelby, on July 4th of all days, the publicity would spark an interest in this otherwise ordinary Hi-Line Town, leading to a nice rush of land sales. Or so the theory went.

As it turned out, things didn't quite work out as planned. First, the offer somehow ended up being increased to \$300,000 through some shrewd negotiations in Chicago by Dempsey's manager, Jack "Doc" Kearns. Then a massive stadium had to be built to accommodate the vast floods of people who were expected to pour into town to see the fight. An octagonal stadium that could seat 42,000 people was built – seemingly overnight, all at great cost. And to house all these people, a huge number of "temporary hotels" were built all up and down Shelby, destined to be torn down following the fight. The Shelby Town Hall was built in less than two months, in part due to the necessity for space for the expected "press men" who would be gathering to cover and record the July 4th extravaganza and to show the world the progress and prosperity of the small hi-line community.

A few things got in the way of prosperity, though. The town had great difficulty in selling advance seats. Moreover, right when it seemed like the town had finally succeeded in booking some advance tickets and organizing the trains to bring people out, the fight seemed like it would not happen, as Shelby wasn't able to come up with the final \$100,000 payment. As such, Dempsey's manager leaked word that the fight would not happen – resulting in the virtual cancellation of all advanced ticket sales. And, by the time word got out that the fight was on again, it was too late to sell tickets and transport all the people out to Shelby.

Thus, on the date of the fight, in a stadium designed to seat 42,000 people, only a handful of people were in attendance. However, there was no shortage of people milling around. Problem was, they were Montana locals, and balked at paying the exorbitant price for admission. Right before the fight the price was lowered drastically, from \$25 to \$10. This persuaded a few people to pay to go in and see the fight. However, not to be left out, the others who refused to pay to see the fight just pushed over the barbed wire that surrounded the stadium right before the opening bell of the first round – leading to a rush of over 4000 people into the stands – none of whom paid. The result of all this was a near empty stadium that was primarily filled with local spectators who were seeing a championship heavyweight boxing match the town of Shelby paid for, all for free.

The fight itself, between Heavyweight Jack Dempsey and challenger Tommy Gibbons, was roundly considered a spectacular fight, as it was the first time anyone had gone the full 15 rounds with Dempsey. But that was the high point.

Following the fight, the town of Shelby was basically broke. With virtually no income received from the fight (since no one came), the town was straddled with debts. Indeed, money was so short IOU's had to be used for payment following the fight. The end result was that four banks in Montana completely went out of business due to the losses they sustained in promoting the fight, including the bank which was owned by James W. "Body" Johnson's father. City council reports don't even show whether Frank Bossuot and J.P. Humphrey were ever paid in full for the design and construction of the town hall. Nonetheless, the building stands as a reminder of the heady days during the spring of 1923, as well as a landmark of local government and the early progress of Shelby, Montana.

Shelby through the Twentieth Century

Much to the credit of area homesteaders, oil producers and businessmen, Shelby did survive the "fight fiasco". The "Town Fathers" quietly paid their debts, journalists, fight fans and curiosity seekers left without a trace, and Shelby returned to the peaceful community it once was. Drought periods came and went and area farmers found their niche in dry land farming. Ranchers found

United States Department of the Interior
National Park Service

National Register of Historic Places
Continuation Sheet

Section number 8

Shelby Town Hall
Toole County, MT

Page 3

grassland for their cattle and sheep, oil production stabilized and area businessmen kept Shelby thriving. The railway system maintained Shelby as a distribution center and the coming of the interstate highway system further identified Shelby as a crossroads for the western region. Throughout these times, the Shelby Town Hall served as government offices and a community meeting area. Removed from much of the worldwide events, Shelby quietly survived depression and wars. The years following saw Shelby's population grow and decline, businesses came and went, but the courage and determination of those early homesteaders remained strong and loyal.

The Shelby Town Hall continued to serve the community for these past 80 years. It served as government offices; a local citizen's meeting hall and even a jail. The upstairs served as the council meeting room as well as the clerk and secretary's office. The mayor had an office in the basement of the building as well as an office for the building inspector. The Town Hall also served as the headquarters for the Shelby Ground Observer Corps in the 1950's. The Ground Observer Corps, dubbed "Operation SKYWATCH", was organized October 20, 1954 at a meeting held at the Capital Café.

The Shelby Ground Observer Corps with its roots to World War II, was part of the 16,000 observation posts positioned along the nation's coasts. With limited radar detection capability, the GOC's mission was to visually search the skies for enemy aircraft attempting to penetrate American airspace. A large crowd attended the meeting and many volunteered to serve as need in what they called, "an important phase of national defense." The Shelby police, sheriff's office, highway patrol, Marias River Electric and the Montana Dakota Utilities Company all pledged to help in reporting planes in the Shelby area through the use of their two-way radio equipped cars and trucks. Sergeant Pettetti and Ferrick of the GOC center in Helena were at the meeting and explained operational matters, such as how to call in a plane flying over the area and how to fill out the aircraft flash message records. The operation would take around 100 volunteers, whose services would be required for only two hours each week, sitting in the observer tower on top of the town hall watching for planes.

Shelby initially had problems recruiting the volunteers it needed, which prompted the GOC president, Tom Crayon to call a special meeting on December 8, 1954, which he called a "wake up and sign up session." Interestingly, the majority of the volunteers were the women of Shelby. A tower was constructed on the roof of the building and manned mostly by these women volunteers. Not much is known as to how long the tower stood on the roof of the building as the deployment of the short-range AN/FPS-14 radar resolved the problem of detecting low-flying planes and disestablished the Ground Observer Corps on January 31, 1959. Despite only being in service for four years, the Shelby Ground Observation Corps played an important and interesting role in the history of Shelby and the Shelby Town Hall.

In 1998, the former Toole County Hospital was renovated and revitalized to create the Marias Heritage Center, an assisted living facility for elderly residents. The facility also became the new offices for the City of Shelby. The Shelby Area Chamber of Commerce took over the old town hall office space and shortly after a Visitor Information Center was established for the benefit of visitors traveling through the area.

Conclusion:

The Shelby Town Hall served as a monument to ingenuity and determination of the town fathers to create a lasting, perpetuating building to serve as a community center, cementing a small group of homesteaders, oilfield workers, and businessmen and their families into a "town". The dirt roads, sparse buildings and single railroad track became a viable, economic community and the Shelby Town Hall served as its headquarters.

Today, the integrity of the building has been maintained and the facility used as a Visitor Information Center, Shelby Area Chamber of Commerce office and additional city office space. The City of Shelby is developing plans to restore the building to its former glory and make it a wonderful stop for visitors to the community recreating the glory of the "oil days" in North Central Montana.

9. Major Bibliographic References

(see continuation sheet)

Previous documentation on file (NPS):

- preliminary determination of individual listing (36 CFR 67) has been requested
- previously listed in the National Register
- previously determined eligible by the National Register
- designated a National Historic Landmark
- recorded by Historic American Buildings Survey # _____
- recorded by Historic American Engineering Record # _____

Primary Location of Additional Data:

- State Historic Preservation Office
 - Other State agency
 - Federal agency
 - Local government
 - University
 - Other
- Specify Repository:

10. Geographical Data

Acreage of Property: less than one

UTM References:	Zone 12	Easting	Northing	
	12	436903	5372509	(NAD27)

Legal Location (Township, Range & Section(s)): Township 32 North, Range 2West, NW ¼ SE ¼ NW ¼ of section 27

Verbal Boundary Description

The Shelby Town Hall is located in the Original Townsite tract of land 349.8 ft. by 103.2 ft. adjacent to U.S. Highway 2. Original - Shelby Unplatted Addition, 1899. This tract is located at the northwest corner of the city owned, Shelby Town Square, abutting Block 2, Lot 1, 2, 4-9 of the Original Townsite.

Boundary Justification

The boundary is drawn, according to legally recorded lines, to include the land historically associated with the Shelby Town Hall.

11. Form Prepared By

name/title: Lorette Carter
organization: City of Shelby date: May 2005
street & number: 112 1st St. So. telephone: (406) 424-8799
city or town: Shelby state: MT zip code: 59474

Property Owner

name/title: City of Shelby
street & number: 112 1st St. So. telephone: (406) 434-5222
city or town: Shelby state: MT zip code: 59474

**United States Department of the Interior
National Park Service**

**National Register of Historic Places
Continuation Sheet**

Section number 9

Shelby Town Hall
Toole County, MT

Page 4

Bibliography

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Johnson, James W. (Body). The Truth About '23. Shelby, Montana: Promoter Publishing, 1966

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Ringside Shelby. A Wheeler Bros. documentary. Montana PBS, 2003

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United States Department of the Interior
National Park Service

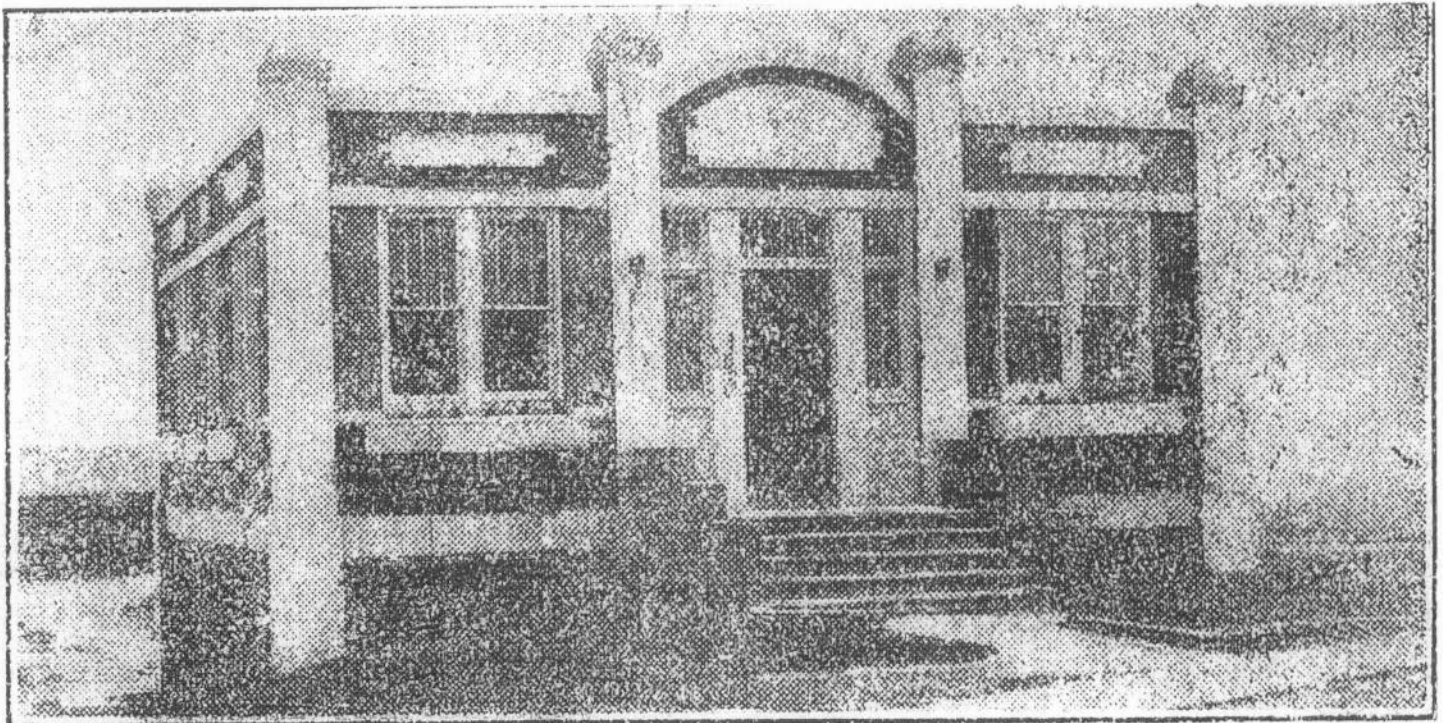
National Register of Historic Places
Continuation Sheet

Photographs

Shelby Town Hall
Toole County, MT

Page 5

Photographs



Shelby Town Hall c. 1924.

United States Department of the Interior
National Park Service

National Register of Historic Places
Continuation Sheet

Plans and Maps

Shelby Town Hall
Toole County, MT

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Shelby Orthophotoquad – photo date 7/20/1991

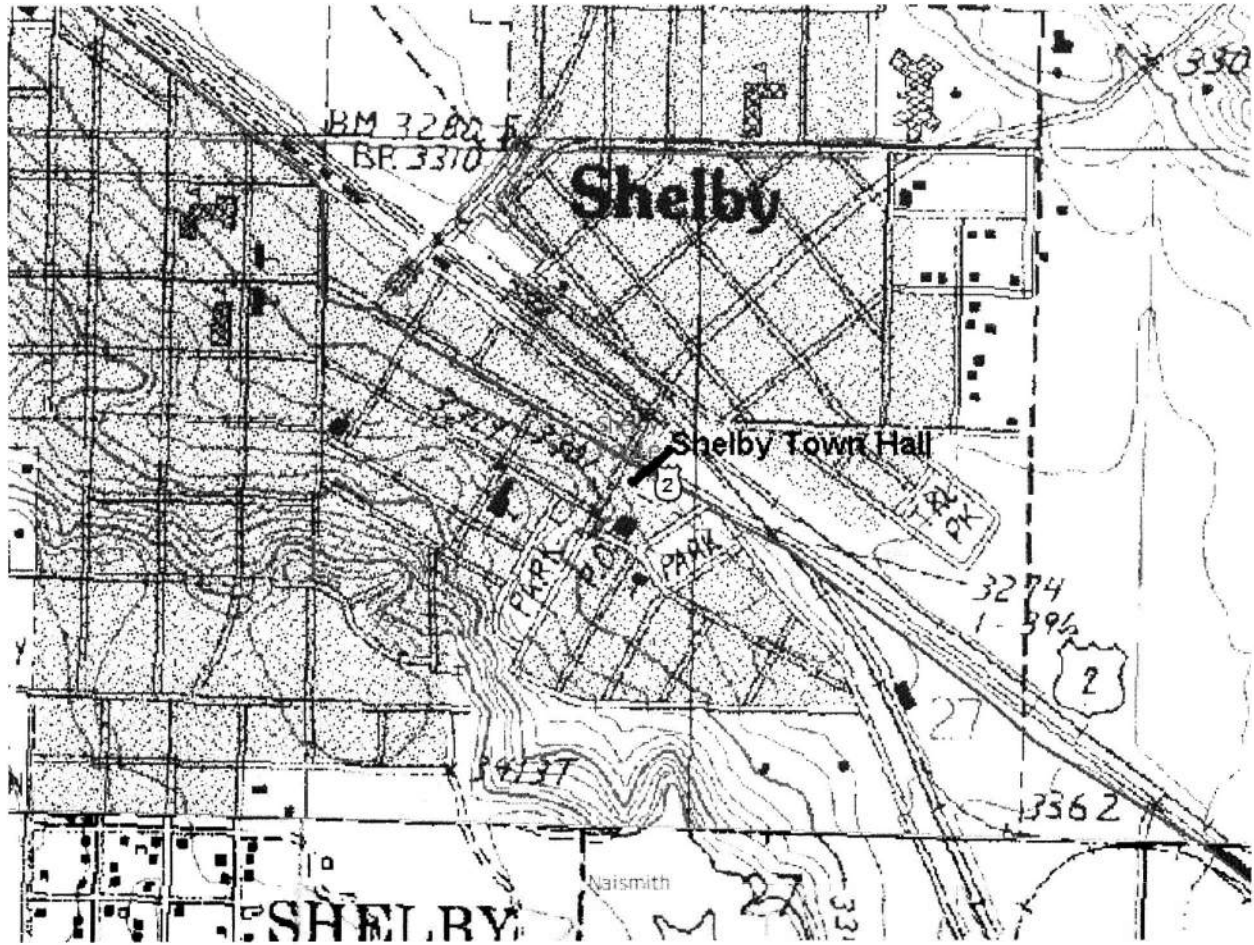
United States Department of the Interior
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Continuation Sheet

Plans and Maps

Shelby Town Hall
Toole County, MT

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Detail of Shelby 7.5 minute Quadrangle

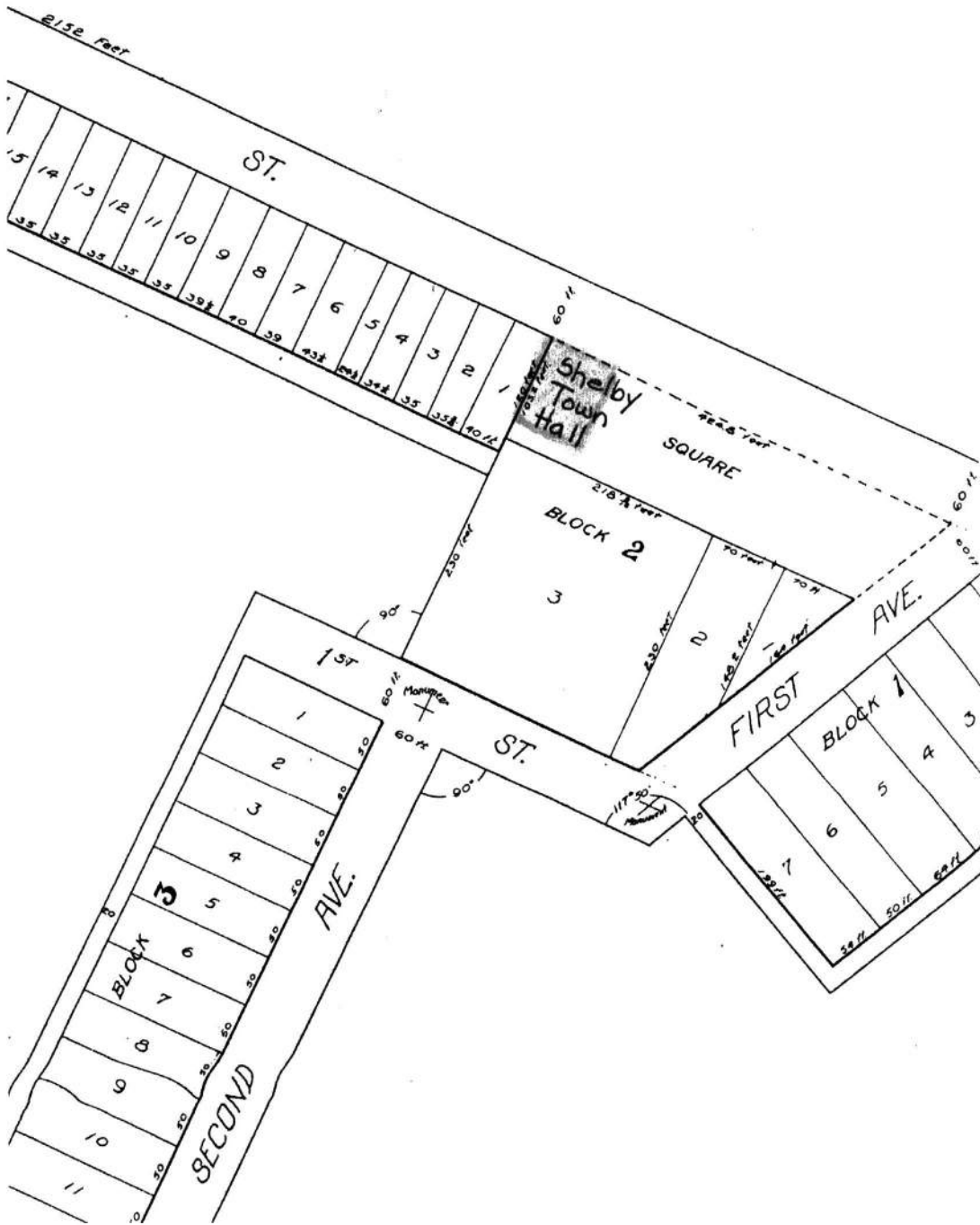
United States Department of the Interior
National Park Service

National Register of Historic Places
Continuation Sheet

Plans and Maps

Shelby Town Hall
Toole County, MT

Page 8



Map of Shelby showing Shelby Town Hall parcel.

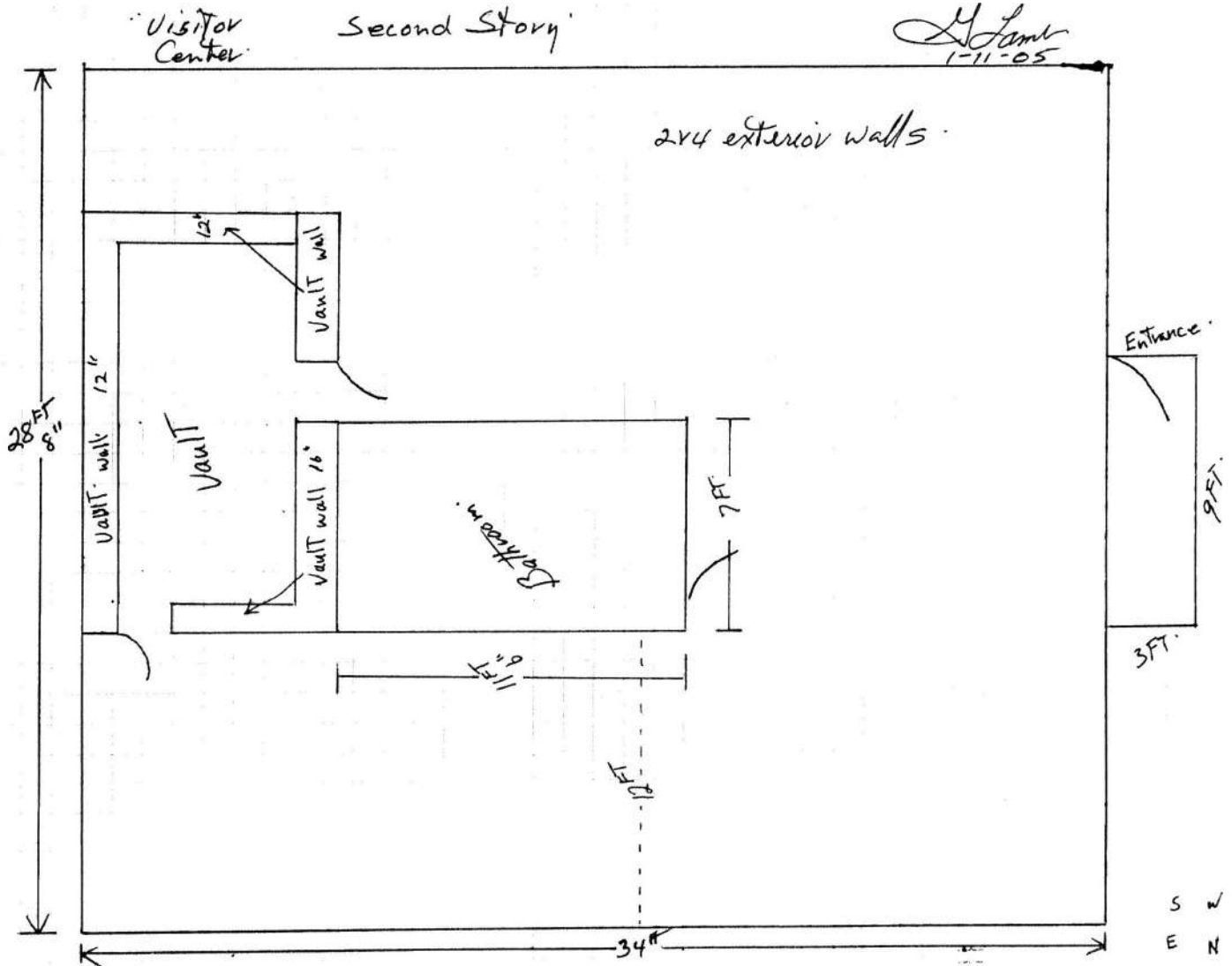
United States Department of the Interior
National Park Service

National Register of Historic Places
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Plans and Maps

Shelby Town Hall
Toole County, MT

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Main Floor plan of Shelby Town Hall

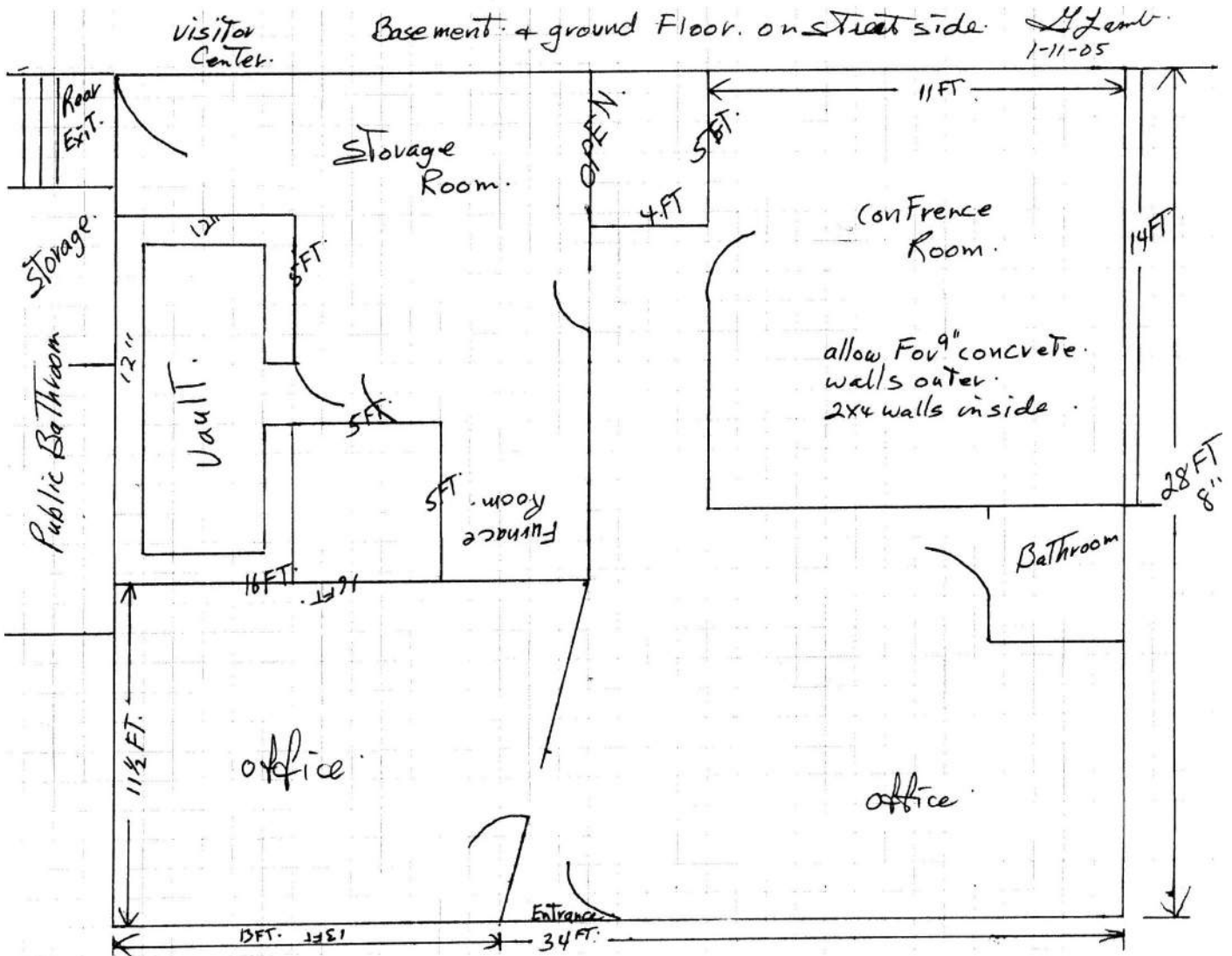
United States Department of the Interior
National Park Service

National Register of Historic Places
Continuation Sheet

Plans and Maps

Shelby Town Hall
Toole County, MT

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Ground Floor/Basement plan of Shelby Town Hall.

United States Department of the Interior
National Park Service

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Continuation Sheet

Plans and Maps

Shelby Town Hall
Toole County, MT

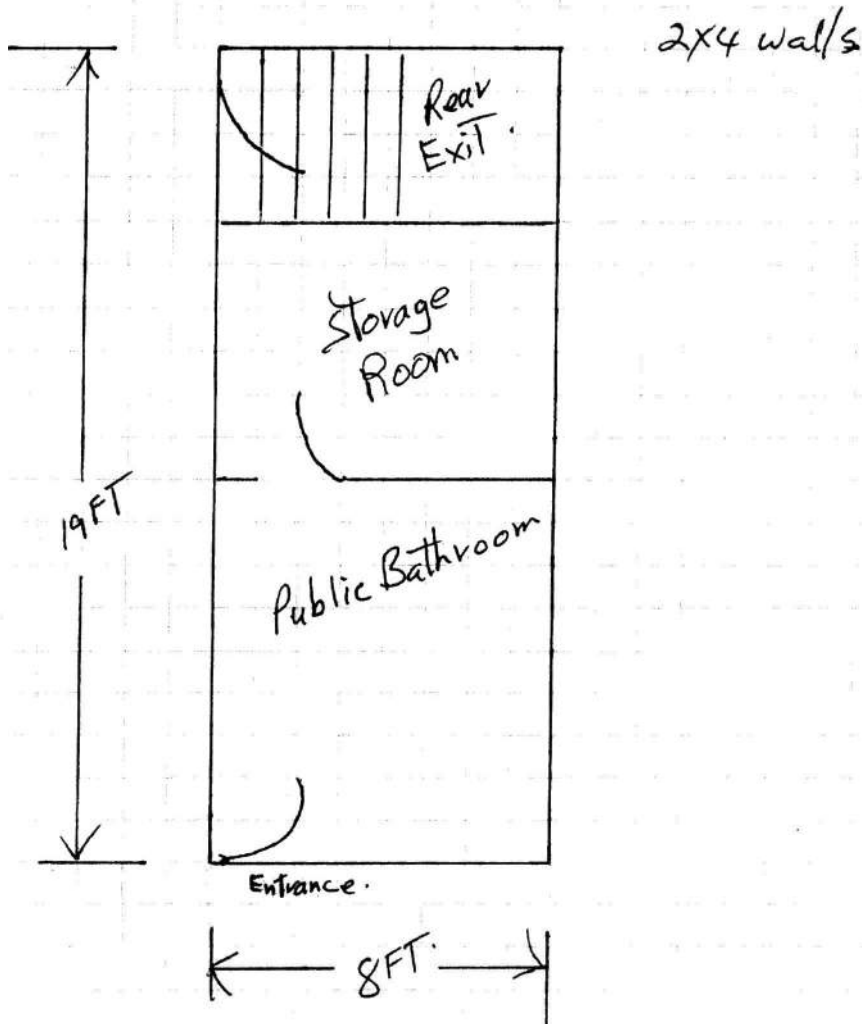
Page 11

Visitor
Center:

addition.

M. Lamb.

1-11-05



East elevation addition plan for Shelby Town Hall.

UNITED STATES DEPARTMENT OF THE INTERIOR
NATIONAL PARK SERVICE

NATIONAL REGISTER OF HISTORIC PLACES
EVALUATION/RETURN SHEET

REQUESTED ACTION: NOMINATION

PROPERTY Shelby Town Hall
NAME:

MULTIPLE
NAME:

STATE & COUNTY: MONTANA, Toole

DATE RECEIVED: 1/06/06 DATE OF PENDING LIST: 1/25/06
DATE OF 16TH DAY: 2/09/06 DATE OF 45TH DAY: 2/19/06
DATE OF WEEKLY LIST:

REFERENCE NUMBER: 06000040

REASONS FOR REVIEW:

APPEAL: N DATA PROBLEM: N LANDSCAPE: N LESS THAN 50 YEARS: N
OTHER: N PDIL: N PERIOD: N PROGRAM UNAPPROVED: N
REQUEST: N SAMPLE: N SLR DRAFT: N NATIONAL: N

COMMENT WAIVER: N

ACCEPT RETURN REJECT 2/14/06 DATE

ABSTRACT/SUMMARY COMMENTS:

Entered in the
National Register

RECOM./CRITERIA _____

REVIEWER _____ DISCIPLINE _____

TELEPHONE _____ DATE _____

DOCUMENTATION see attached comments Y/N see attached SLR Y/N

If a nomination is returned to the nominating authority, the nomination is no longer under consideration by the NPS.



SHELBY Area Chamber of Commerce

VISITOR
INFORMATION
CENTER

Chamber of Commerce

Shelby Town Hall - west elevation (front)

Toole County MT

Photographed: Lorette Carter

Date: June 2005

Location of negatives: MT SHPO

Photo # 1

View to: E



Shelby Town Hall - north elevation

Toole County MT

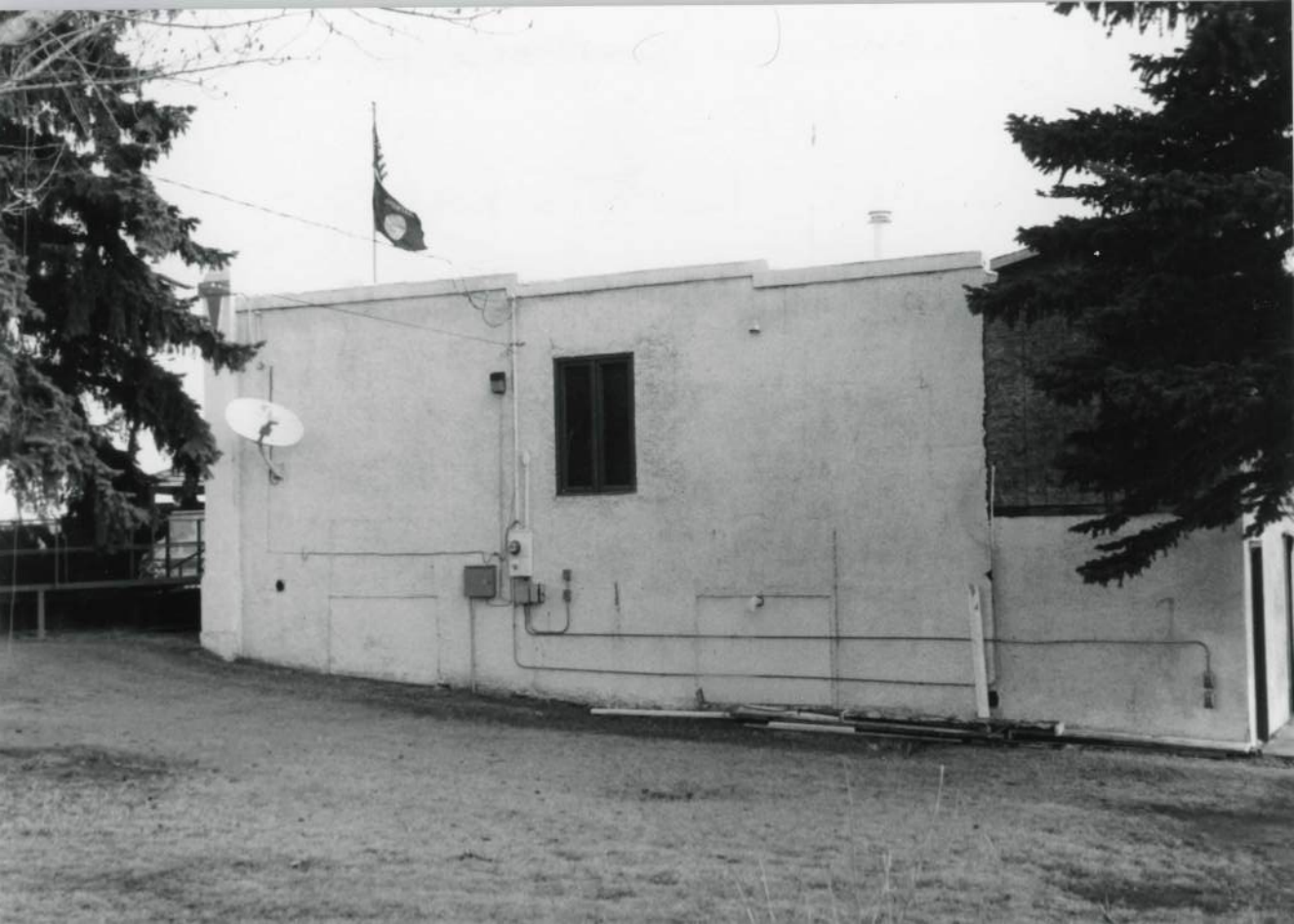
Photographer: Lorette Carter

Date: June 2005

Location of negative: MT SHPO

View to: SE

Photo # 2



Shelby Town Hall - south elevation

Toole County MT

Photographer: Lorette Carter

Date: ~~July 2005~~ June 2005

Location of negative: MT SH PO

View to N

Photo # 3



Shelby Town Hall - east end of south elevation
Toole County, MT

Photographer: Lorette Carter

Date: June 2005

Location of negatives: MT SHAPO

View to: N

Photo # 4



Shelby Town Hall - east elevation
Toole County, MT

Photographer: Lorette Carter

Date: ~~July~~ 2005

Location of negatives: MT SHPO

View to west

Photo # 5

SHELBY Area Chamber of Commerce

**VISITOR
INFORMATION
CENTER**



Shelby Town Hall - north end of east addition

Toole County MT

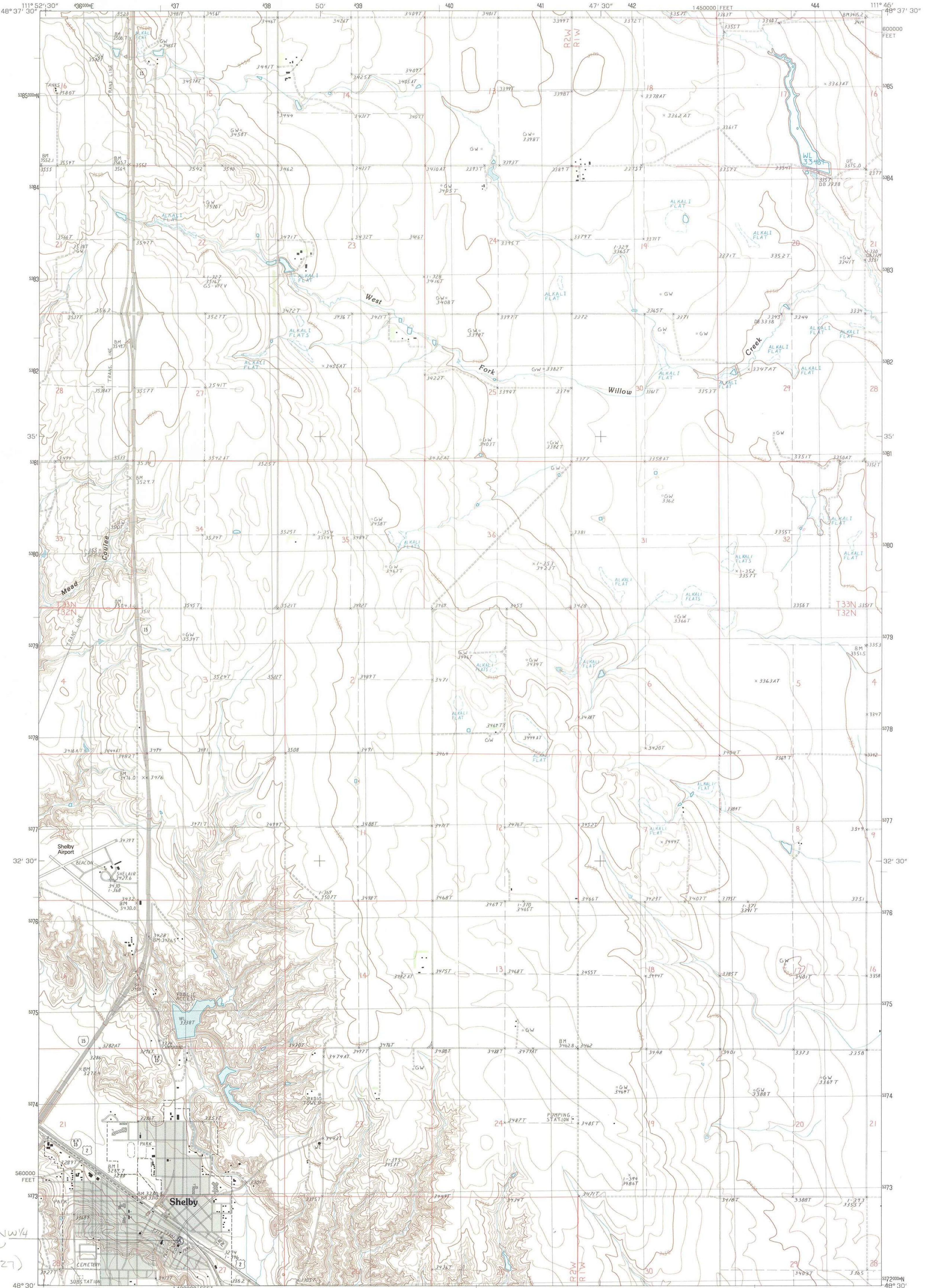
Photographer: Lorette Carter

Date: ~~July~~ 2005 July 2005

Location of negatives: MT SHPO

View to: SW

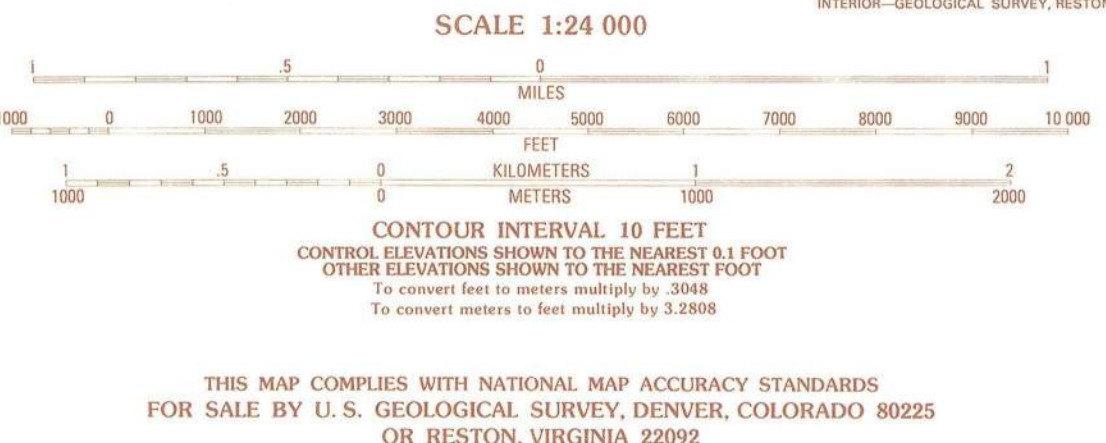
Photo #: 6



Shelby Town Hall
Toole County, MT
Located in NW 1/4 SE 1/4 NW 1/4
sec 27 T32N R2W
UTMs: Zone 12 (NAD27)
436903E 5372509N

PRODUCED BY THE UNITED STATES GEOLOGICAL SURVEY
CONTROL BY USGS AND NOS/NOAA
COMPILED FROM AERIAL PHOTOGRAPHS TAKEN 1983
FIELD CHECKED 1984. MAP EDITED 1986
PROJECTION LAMBERT CONFORMAL CONIC
GRID: 1000-METER UNIVERSAL TRANSVERSE MERCATOR ZONE 12
10,000-FOOT STATE GRID TICKS MONTANA, NORTH ZONE
UTM GRID DECLINATION 0°37' WEST
1986 MAGNETIC NORTH DECLINATION 18°00' EAST
VERTICAL DATUM NATIONAL GEODETIC VERTICAL DATUM OF 1929
HORIZONTAL DATUM 1927 NORTH AMERICAN DATUM
To place on the predicted North American Datum 1983,
move the projection lines as shown by dashed corner ticks
(8 meters north and 62 meters east)
There may be private inholdings within the boundaries of any
Federal and State reservations shown on this map
No distinction made between houses, barns, and other buildings
Gray tint indicates area in which selected buildings are shown

PROVISIONAL MAP
Produced from original
manuscript drawings. Informa-
tion shown as of date of
photography.



QUADRANGLE LOCATION

1	2	3
4	5	6
7	8	9

ADJOINING 7.5 QUADRANGLE NAMES

1	2	3
4	5	6
7	8	9

1 Kulis South
2 Oilmont
3 Antelope Coulee NW
4 Vidson
5 Antelope Coulee SW
6 Ledger NW
7 Namouth
8 Dunkirk

ROAD LEGEND

Improved Road
Unimproved Road
Trail
Interstate Route U.S. Route State Route

SHELBY, MONTANA
PROVISIONAL EDITION 1986

48111-E7-TF-024



MONTANA HISTORICAL SOCIETY

225 North Roberts ♦ P.O. Box 201201 ♦ Helena, MT 59620-1201
♦ (406) 444-2694 ♦ FAX (406) 444-2696 ♦ www.montanahistoricalsociety.org ♦



January 4, 2006

Janet Matthews, Keeper
National Register of Historic Places
National Park Service
1201 Eye St., NW
8th Floor (MS 2280)
Washington, DC 20005

Dear Dr. Matthews,

Enclosed please find the following nomination for your consideration for listing in the National Register of Historic Places.

Shelby Town Hall **Toole County, MT**

Please be advised that I submit the enclosed nomination under your revised procedures. I have notified the owners and public officials in excess of 30 days prior to the Preservation Review Board meeting.

The Review Board unanimously recommended that this property be nominated and I concur with their recommendation.

Thank you for your consideration.

Sincerely,

Stan Wilmoth, PhD
Acting State Historic Preservation Officer

Enclosures

